

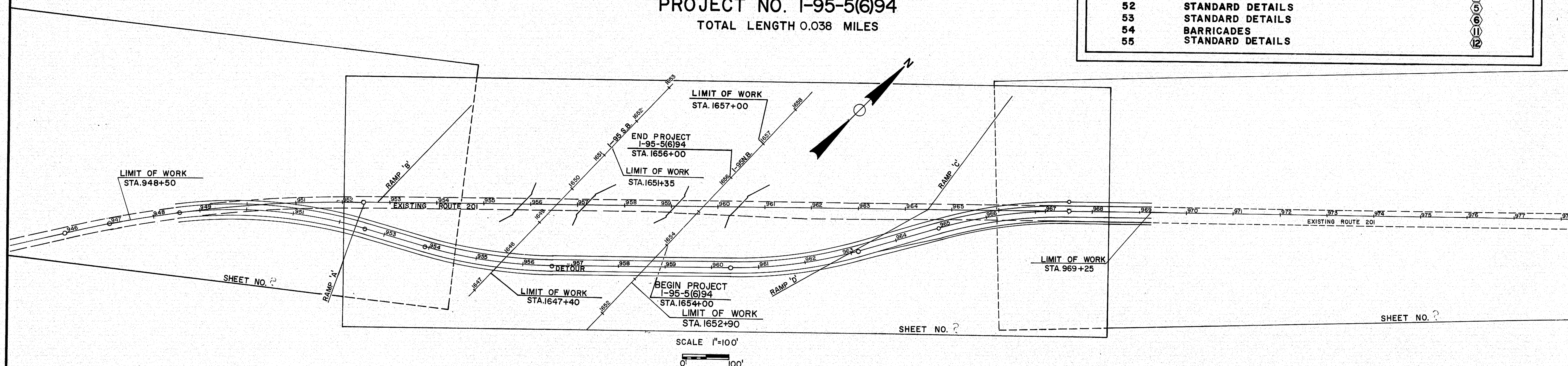
STATE OF MAINE STATE HIGHWAY COMMISSION



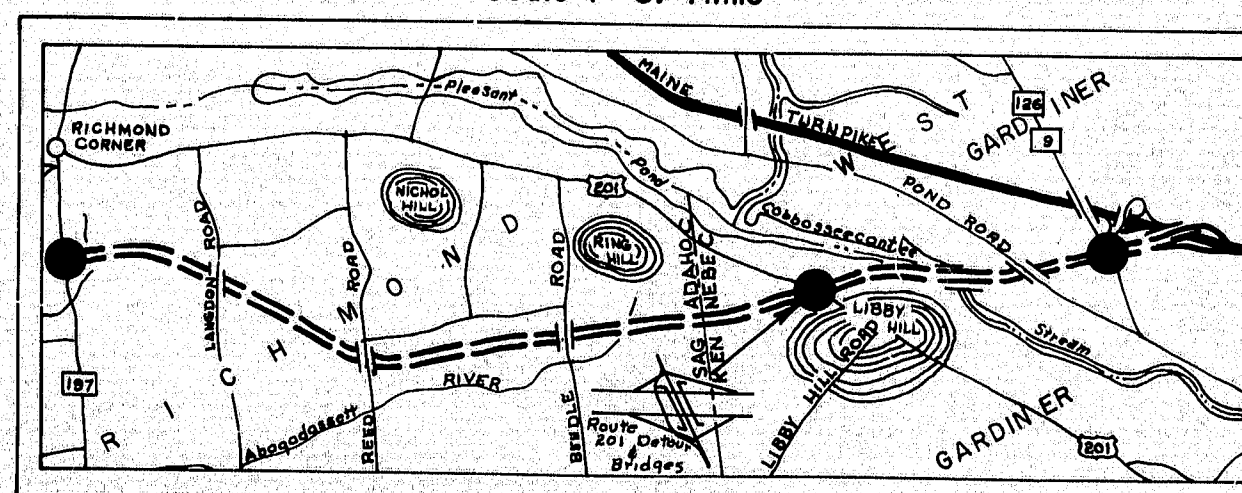
ROUTE 201 BRIDGES OVER INTERSTATE 95 IN THE CITY OF GARDINER KENNEBEC COUNTY MAINE FEDERAL AID INTERSTATE PROJECT NO. I-95-5(6)94 TOTAL LENGTH 0.038 MILES

CONVENTIONAL SIGNS	
COUNTY LINES	TRAVELLED WAY - PROPOSED
TOWN LINES	UNDERGROUND UTILITIES - EXISTING
PROPERTY LINES	UNDERGROUND UTILITIES - PROPOSED
R/W LINES - EXISTING	RAILROAD - SINGLE TRACK
R/W LINES - NEW - ACCESS CONTROL	RAILROAD - DOUBLE TRACK
R/W LINES - NEW - NO ACCESS CONTROL	UTILITY POLE - EXISTING
CULVERT - EXISTING	UTILITY POLE - JOINT OCCUPANCY
CULVERT - PROPOSED	PROPOSED UTILITY POLE - TEMPORARY
CURBING - EXISTING	PROPOSED UTILITY POLE - PERMANENT
CURBING - PROPOSED	TREES
TRAVELLED WAY - EXISTING	WOODS

INDEX OF SHEETS	
SHEET	CONTENTS
1	TITLE SHEET
2	QUANTITIES
3	GENERAL PLAN
4	PROFILE ROUTE 201
5-6	RIGHT OF WAY
7-8	SOILS
9	ARCHITECTURAL TREATMENT
10-21	ABUTMENTS
22-24	STRUCTURAL STEEL
25	BLOCKING
26	SUPERSTRUCTURE
27	ARMORED JOINT
28	APPROACH SLABS AND RAIL DETAILS
29-30	SLOPE PROTECTION
31	REINFORCING STEEL SCHEDULE
32-33	DETOUR PLAN
34-40	DETOUR CROSS SECTION
41	TYPICAL SECTIONS OF I 95 AND DRAINAGE
42-46	CROSS SECTIONS I 95
47	BEARING PEDESTALS (BD 101-70)
48	ARMORED JOINTS, SHEAR CONNECTORS, DRAINS, (BD 104-71)
49	ALUMINUM RAILING (BD 106-69)
50	STANDARD DETAILS
51	STANDARD DETAILS
52	STANDARD DETAILS
53	STANDARD DETAILS
54	BARRICADES
55	STANDARD DETAILS



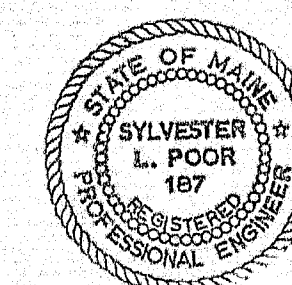
A PORTION OF SAGadahoc AND KENNEBEC COUNTIES
Scale 1"= 3/4 Mile



NOTE:
All work contemplated under this contract to be governed by and in conformity with The Standard Specifications (revisions of June 1968) and supplementals thereto, except as modified on the plans and in the special provisions.

TRAFFIC DATA

I-95		RTE. 201 DETOUR		RTE. 201	
A.D.T.	1974 10240	A.D.T.	1971 3460	A.D.T.	1972 1990
A.D.T.	1994 19210	A.D.T.	1973 3630	A.D.T.	1992 3110
D.H.V.	2497	D.H.V.	472	D.H.V.	404
T.(%)	11	T.(%)	17	T.(%)	11
D.(%)	60	D.(%)	60	D.(%)	60
V.	70	V.	35	V.	60
P.S.D.(%)	100	P.S.D.(%)	N/A	P.S.D.(%)	N/A
		18 KIPS	348	18 KIPS	69



APPROVED:
MAINE STATE HIGHWAY COMMISSION
David H. [Signature]
CHAIRMAN
Steven D. [Signature]
CHIEF ENGINEER

DATE
17 NOV. 1971
17 NOV. 1971
17 NOV. 1971

AS BUILT PLANS W.C. FERRARO JR. 3/5/74
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 1
APPROVED:
DIVISION ENGINEER DATE

GARDINER I-95-5(6)94

162-33

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	QUANTITY	UNIT
201.11	Clearing	4.2	Acre
203.20	Common Excavation	19,920	Cu. Yd.
203.21	Rock Excavation	7490	Cu. Yd.
203.211	Presplitting Rock	2120	L.F.
203.24	Common Borrow	6000	Cu. Yd.
203.25	Granular Borrow	1200	Cu. Yd.
206.06	Str. Earth Excav. - Drainage & Minor Strs.	182	Cu. Yd.
206.08	Str. Earth Excav. - Abuts. & Ret. Walls	2000	Cu. Yd.
206.09	Str. Rock Excav. - Abuts. & Ret. Walls	125	Cu. Yd.
304.10	Aggregate Subbase Course - Gravel	4520	Cu. Yd.
403.07	Hot Bit. Pavement, Grading B	875	Ton
403.121	Hot Bituminous Pavement, Grading E (Shimming)	20	Ton
502.21	Structural Concrete, Abuts. & Retaining Walls	1880	Cu. Yd.
* 502.26	Str. Conc., Rdwy. & Sidewalk Slabs on Steel Br.	1	L.S.
* 502.29	Str. Conc., Wearing Surface on Bridges	1	L.S.
503.12	Reinforcing Steel, Fab. & Delivered	136,500	Lb.
503.13	Reinforcing Steel, Placing	136,500	Lb.
* 504.70	Structural Steel, Fab. & Delivered	1	L.S.
* 504.71	Structural Steel, Erection	1	L.S.
504.74	Metal Inserts	104	Each
* 505.08	Shear Connectors	1	L.S.
* 506.14	Field Painting, Structural Steel	1	L.S.
507.08	Bridge Railing	642	L.F.
512.07	French Drains (Stones Only)	79	Cu. Yd.
514.06	Curing Box for Concrete Cylinders	1	Each
515.20	Protective Coating for Concrete Surfaces	1490	Sq. Yd.
603.16	15 inch Culvert Pipe, Option I	170	L.F.
603.17	18 inch Culvert Pipe, Option I	48	L.F.
603.20	30 inch Culvert Pipe, Option I	100	L.F.
604.09	Catch Basin, Type B1	1	Each
604.14	30 inch Catch Basins, Type E	1	Each
606.17	Guard Rail Type 3b - Single Rail	400	L.F.
606.22	Guard Rail Type 3b - Circular - Greater 15 ft radius	50	L.F.
606.26	Terminal Ends - Single Rail	2	Each

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	QUANTITY	UNIT
609.13	Vertical Bridge Curb - Type 1	556	L.F.
616.08	Sodding	250	Sq. Yd.
617.09	Erosion Control Mesh	700	Sq. Yd.
618.14	Seeding, Method Number 2	125	Unit
619.09	Hay Mulch	125	Unit
629.05	Labor, Straight Time	10	M. Hr.
630.06	Traffic Officers	40	M. Hr.
631.13	Bulldozer (inc. operator)	10	Hour
631.171	Truck - small (including operator)	20	Hour
631.22	Front End Loader (inc. operator)	10	Hour
632.08	Warning Lights	2	Group
633.11	Oversized Portable Barricade with Flashing Lights	4	Each
637.07	Sprinkling	50	M. G.
637.08	Calcium Chloride	5	Ton
639.08	Field Office, Type A	1	Each
639.11	Testing Facilities, Soils	1	L.S.
639.12	Testing Facilities, Bit. Mixes	1	L.S.
639.13	Testing Facilities, Bit. Lig. & Cement	1	L.S.
657.20	Seed & Application, Method A	10	Unit
657.21	Reforestation, Method B	0.25	Acre
660.20	On-The-Job Training	1000	M.H.
	Note:		
	* These Lump Sum Items include		
	both bridges and shall be bid		
	as 1 lump sum for both bridges		

Estimated Quantities of Lump Sum Items

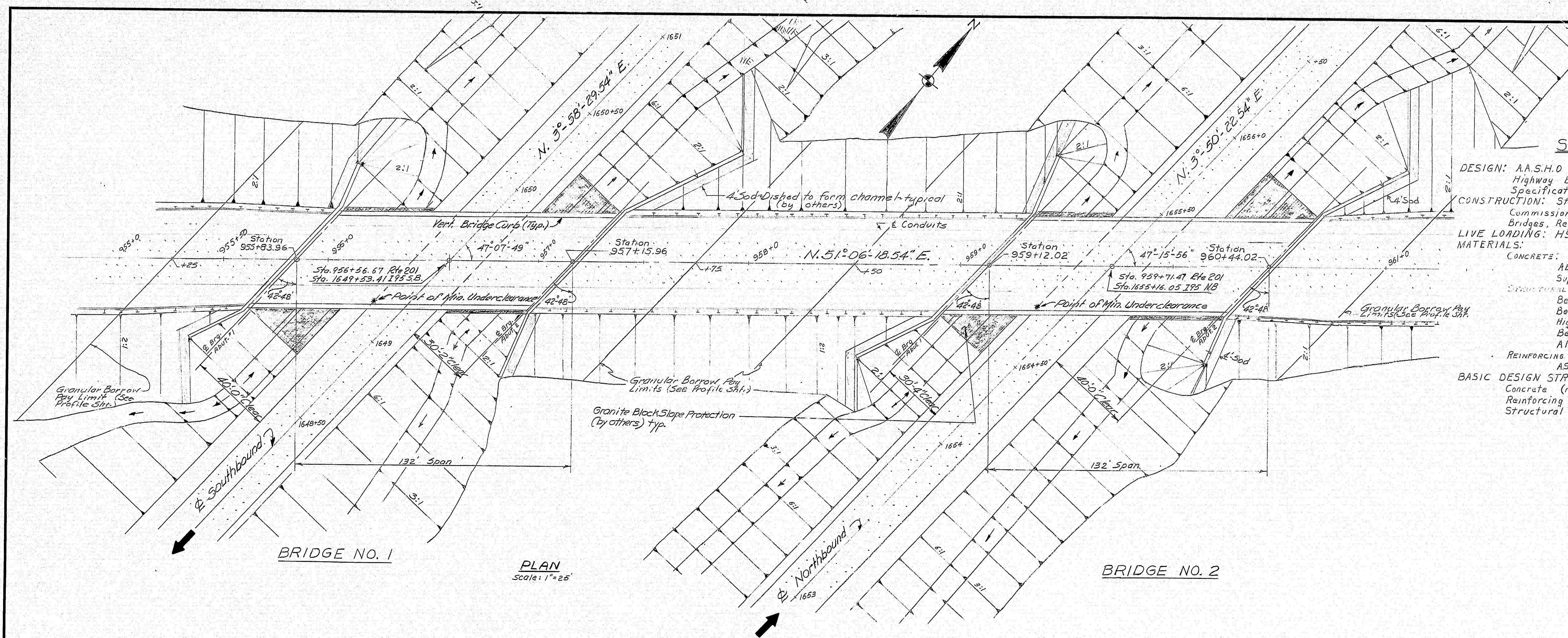
Str. Conc., Rdwy. & Sidewalk Slabs on Steel Br 390 Cu Yds. (Includes Both Superstructures) Item 502.26
 Str. Conc., Wearing Surface on Bridges 107 Cu Yds. (Includes Both Superstructures) Item 502.29
 Structural Steel 487,000 Lbs. Items 504.70, 504.71, & 506.14.
 Shear Connectors 1968 Pieces = 1920 Lbs. Item 505.08

STATE HIGHWAY COMMISSION
 As Bids Plus N.C. Engineers, Inc. 3/6/74
U.S. ROUTE NO. 201
 OVER
INTERSTATE NO. 95 N.B. & S.B.
 IN THE CITY OF
GARDINER
KENNEBEC COUNTY
 QUANTITIES
 SHEET 2 OF 55 AUGUSTA, MAINE Dec. 1971

152-34

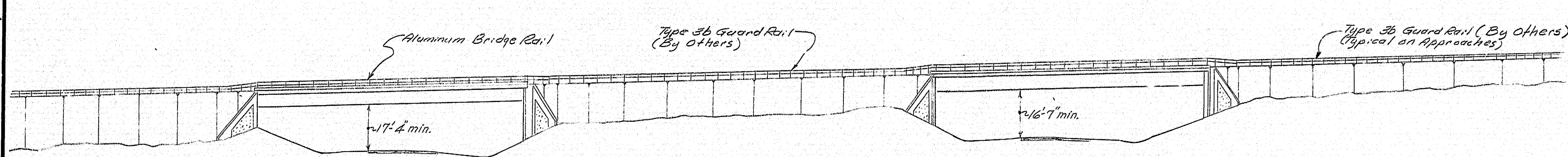
2004 2000

R. P. N.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I.95 S(6)94	3	55



SPECIFICATIONS

DESIGN: A.A.S.H.O. Standard Specifications for Highway Bridges 1969 and Interim Specifications 1970 & 1971.
 CONSTRUCTION: State of Maine, State Highway Commission, Standard Specifications, Highways and Bridges, Revision of June 1968.
 LIVE LOADING: HS20-44.
 MATERIALS:
 CONCRETE:
 Abutments --- Class A
 Superstructure --- Class A
 Reinforcing Steel: --- ASTM Designation
 Beam Webs --- A572 Grade 50
 Beam Flanges --- A572 Grade 50
 High Strength Bolts --- ASTM A-325
 Bearing Stiffeners --- A36
 All Other --- A36
 REINFORCING STEEL:
 ASTM A615 Grade 60
 BASIC DESIGN STRESSES:
 Concrete (n=10) 1200 psi
 Reinforcing Steel 24,000 psi
 Structural Steel 27,000 psi A572
 20,000 psi A36



ELEVATION

Scale: 1" = 25'
 Looking North Along E of Interstate 95

GENERAL NOTE:

This plan shows the completed project.
 This contract is a stage construction contract.
 Work under this contract consists basically of construction of a detour road, earth excavation, presplitting and excavation of ledge for the structures and for I 95 in the area of the bridges, and construction of the bridge structures. Embankment construction for Route 201 shall be limited to that required to dispose of the above mentioned excavation. Abutments shall be backfilled under this contract, up to the limits specified on the Route 201 profile sheet, as directed by the Engineer.

NOTE: PERMANENT DETOUR
 THE 201 DETOUR WILL NOT BE
 REMOVED UNDER THIS CONTRACT.

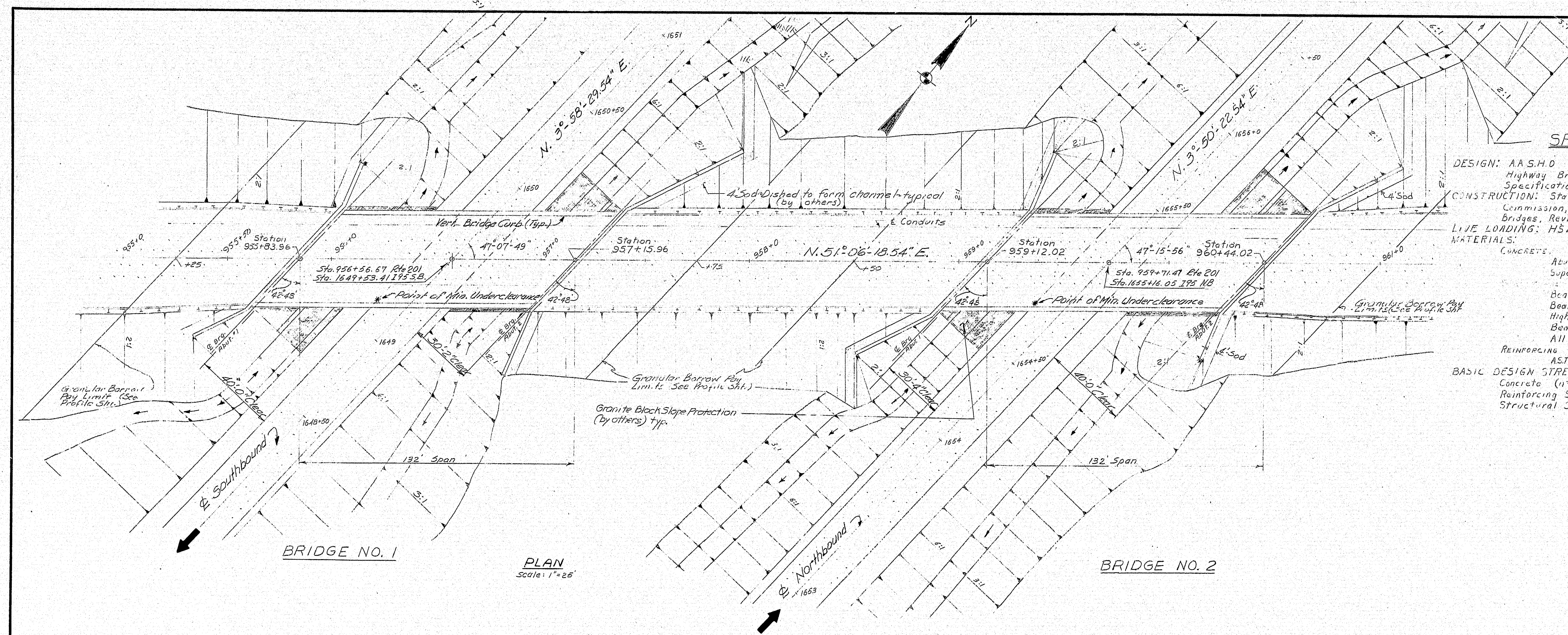
PLANS	DESIGN - DETAIL	BY	DATE
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2	2	ALL	4-77
3	3	ALL	4-77
4	4	ALL	4-77
5	5	ALL	4-77
6	6	ALL	4-77
7	7	ALL	4-77
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33	33	ALL	4-77
34	34	ALL	4-77
35	35	ALL	4-77

STATE HIGHWAY COMMISSION
 U.S. ROUTE NO. 201
 OVER
 INTERSTATE NO. 95 N.B. & S.B.
 IN THE CITY OF
 GARDINER
 KENNEBEC COUNTY
 GENERAL PLAN
 SHEET 3 OF 52 AUGUSTA, MAINE APRIL 1971

152-35



S.P.R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-5(6)94	3	55



SPECIFICATIONS

DESIGN: A.A.S.H.O. Standard Specifications for Highway Bridges 1969 and Interim Specifications 1970 & 1971.

CONSTRUCTION: State of Maine, State Highway Commission, Standard Specifications, Highways and Bridges, Revision of June 1968.

LIVE LOADING: HS20-44.

MATERIALS:

CONCRETE:

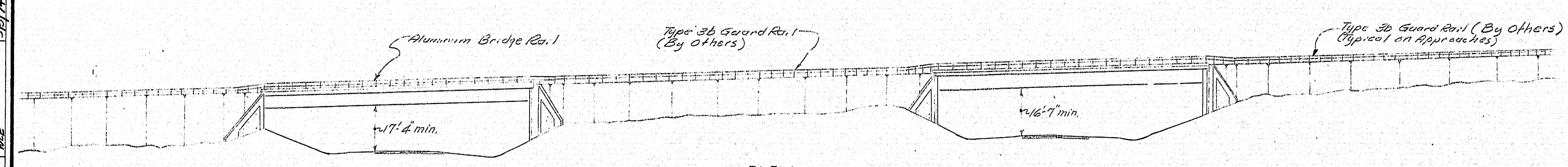
- Abutments - - - Class A
- Superstructure - - - Class A
- Beam Webs - - - A572 Grade 50
- Beam Flanges - - - A572 Grade 50
- High Strength Bolts - - - ASTM A-325
- Bearing Stiffeners - - - A36
- All Other - - - - - A36

REINFORCING STEEL:

- ASTM A615 Grade 60

BASIC DESIGN STRESSES:

- Concrete (f'_c) 4000 psi
- Reinforcing Steel 60,000 psi
- Structural Steel 50,000 psi A572
- 20,000 psi A36



ELEVATION
Scale: 1" = 25'

NOTE: PERMANENT DETOUR
THE DETOUR WILL NOT BE
REMOVED UNDER THIS CONTRACT.

GENERAL NOTE:

This plan shows the completed project. This contract is a stage construction contract. Work under this contract consists basically of construction of a detour road, earth excavation, presplitting and excavation of ledge for the structures and for I-95 in the area of the bridges, and construction of the bridge structures. Embankment construction for Route 201 shall be limited to that required to dispose of the above mentioned excavation. Abutments shall be backfilled under this contract, up to the limits specified on the Route 201 profile sheet, or as directed by the Engineer.

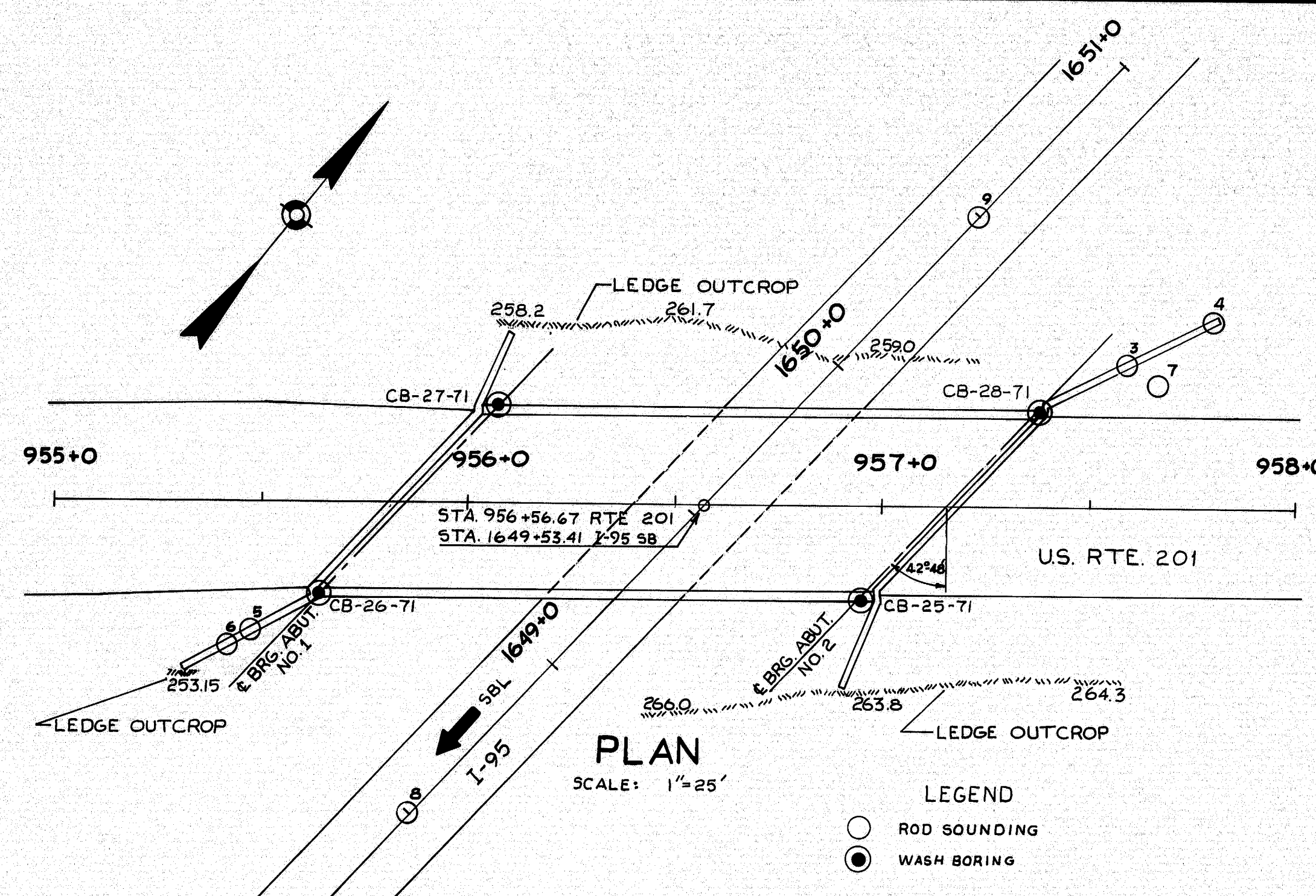
ORIGINAL SHEET USED IN
I-95-5(3) PAVING PROJECT 5(26)

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
GENERAL PLAN
SHEET 3 OF 52 AUGUSTA, MAINE APRIL 1971

152-36

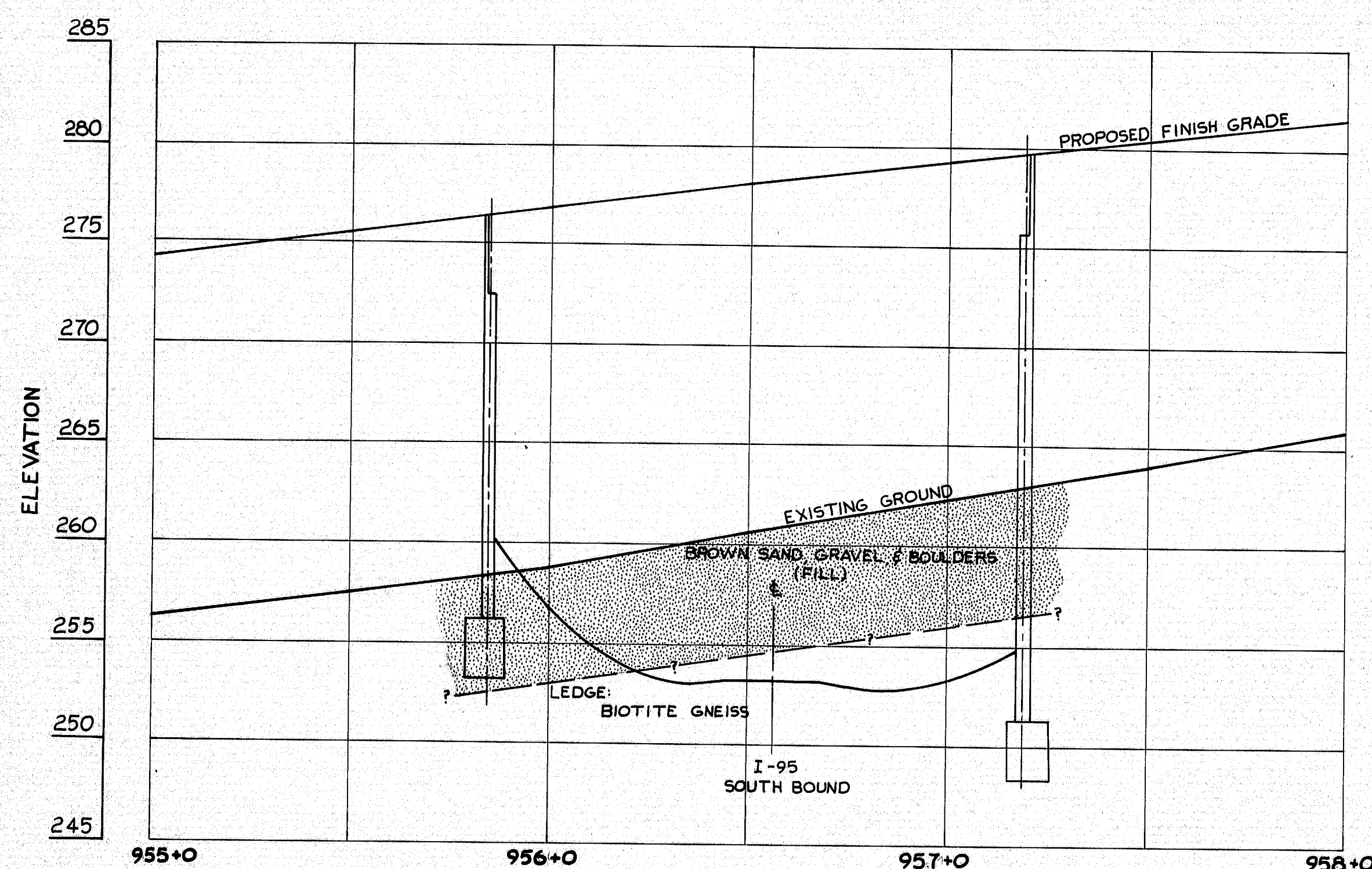
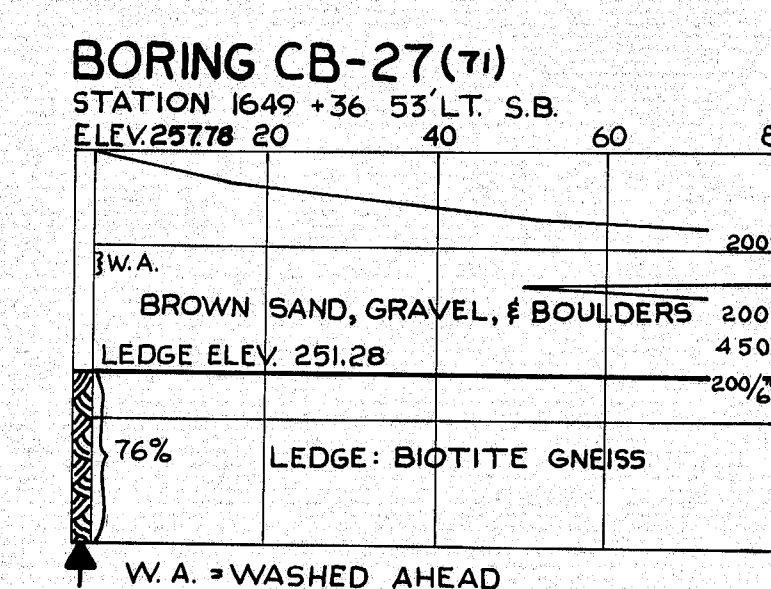
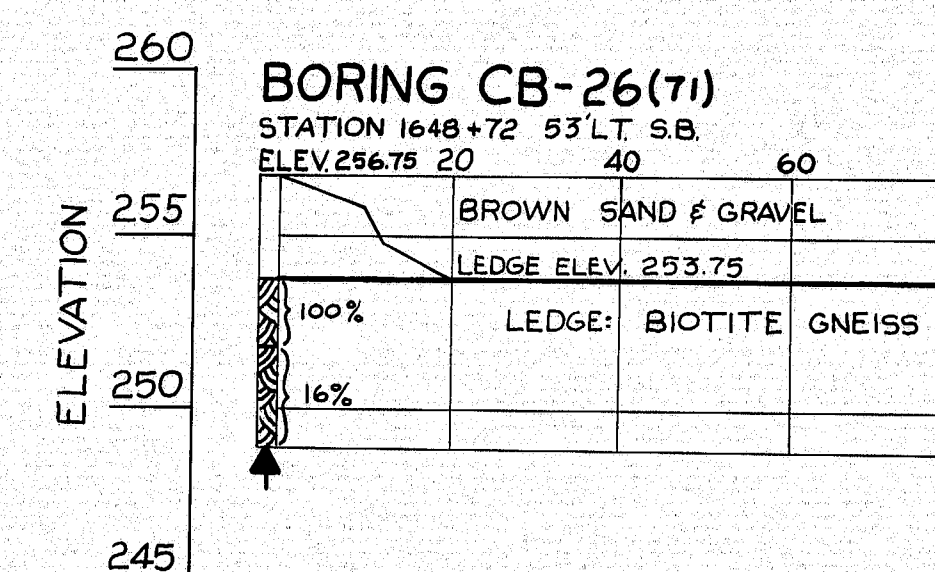
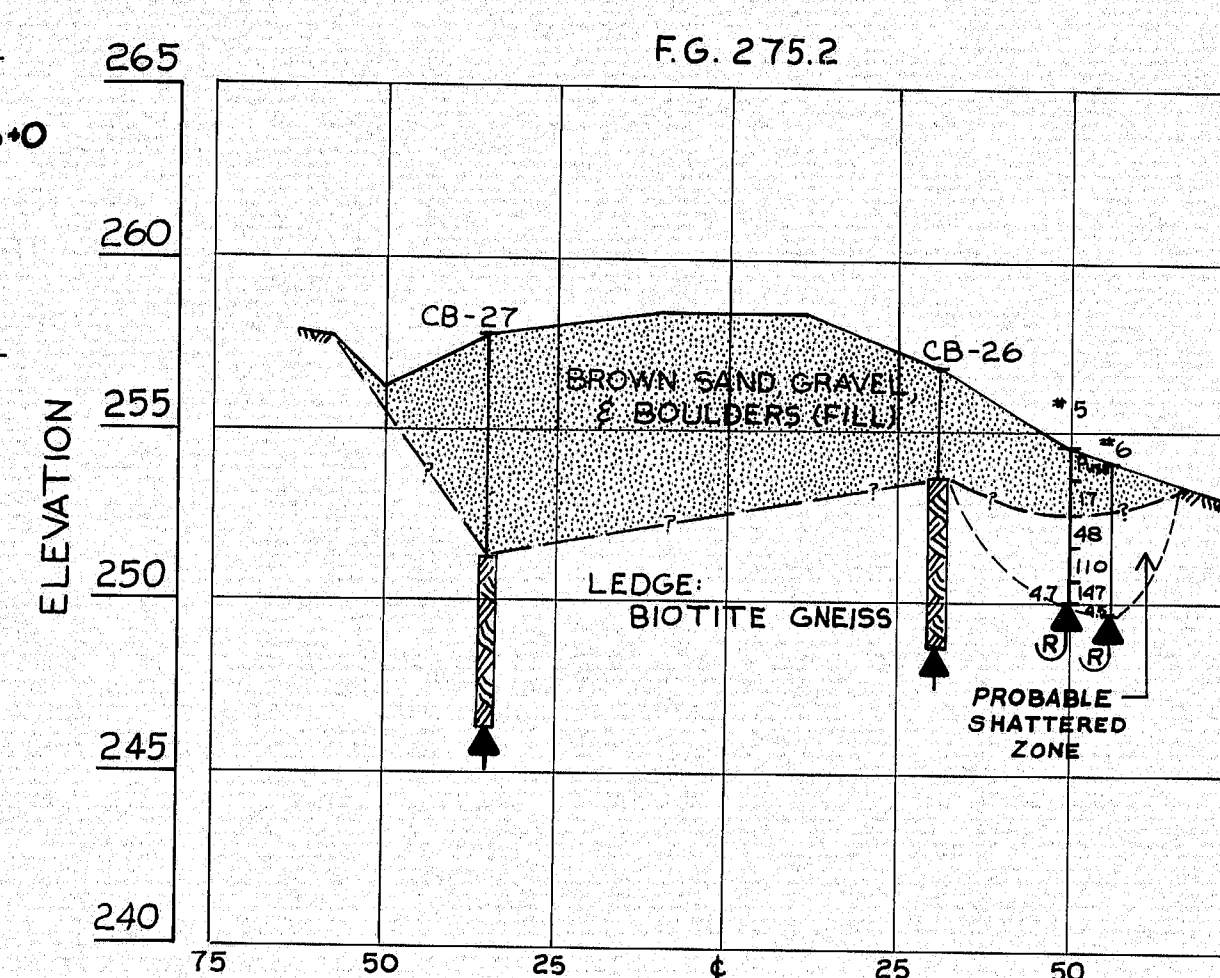
PLANS	DESIGN - DETAIL	CHECKED	REVISIONS	FIELD CHANGES	DATE
ALL	ALL	ALL	ALL	ALL	4-71
REVISIONS	REVISIONS	REVISIONS	REVISIONS	REVISIONS	5/6/74
FIELD CHANGES	FIELD CHANGES	FIELD CHANGES	FIELD CHANGES	FIELD CHANGES	

R. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195 5(6) 94	7	55

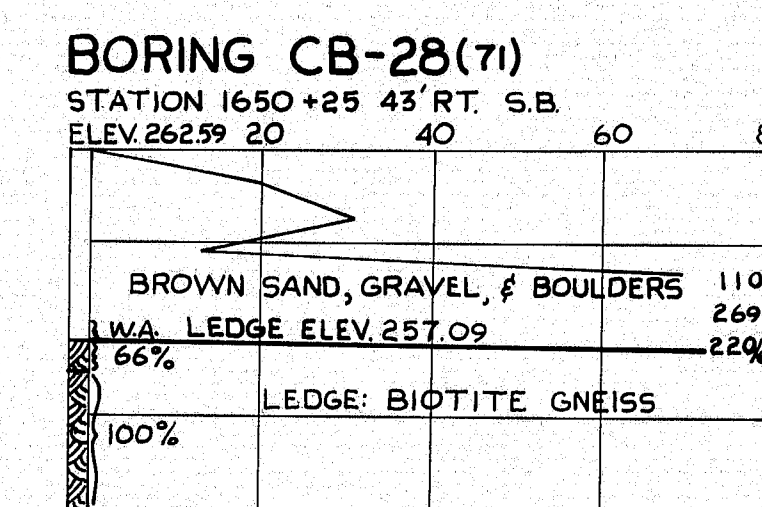
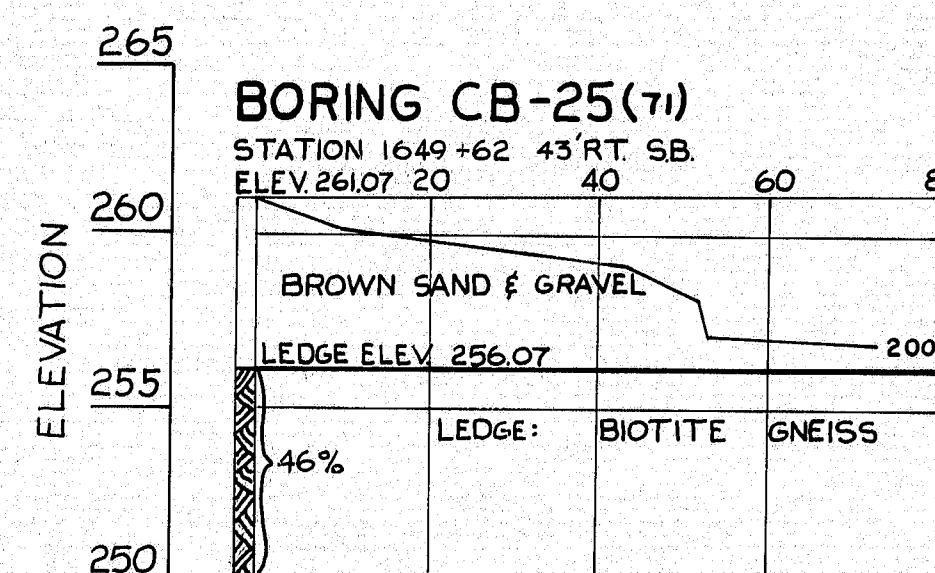
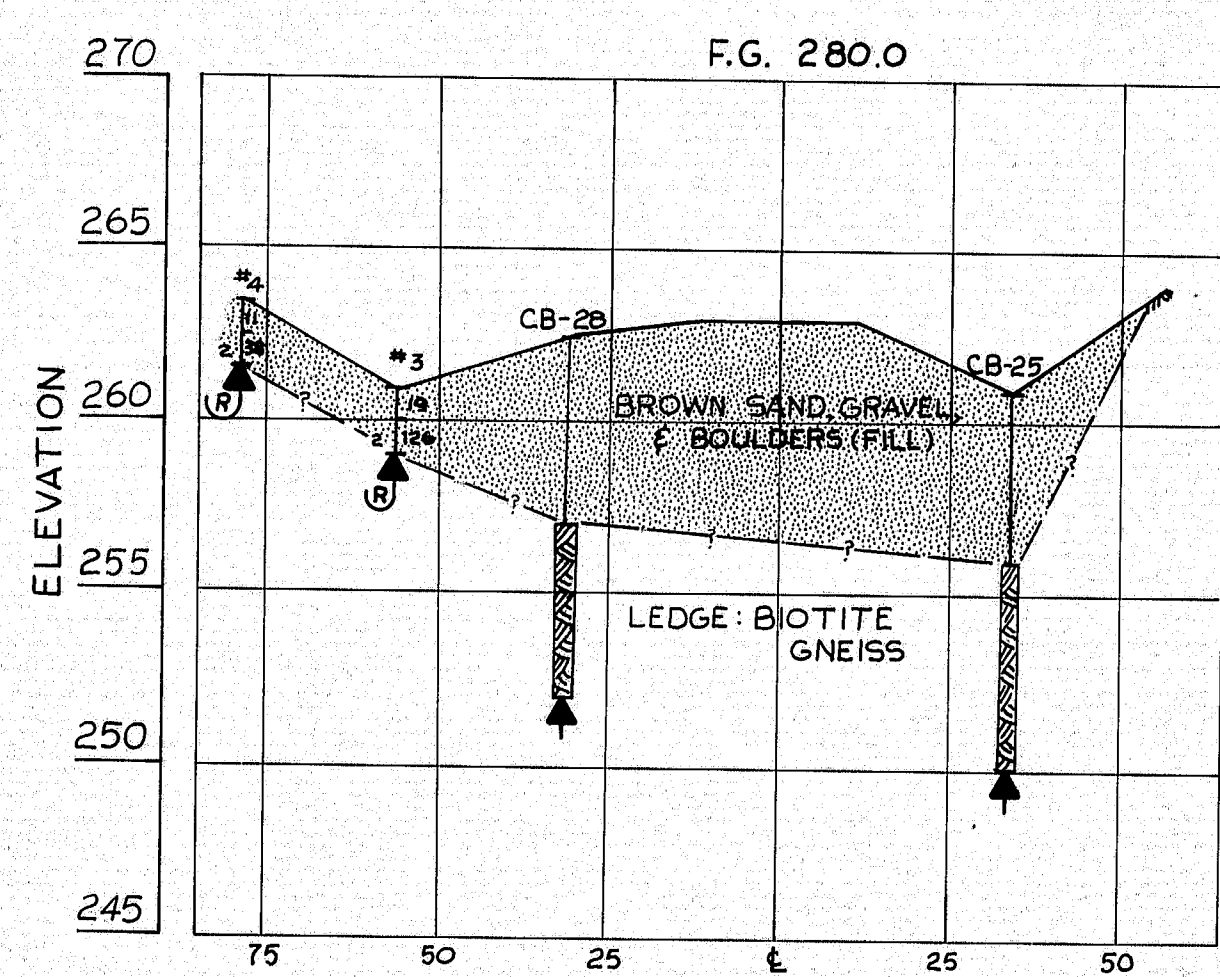


TRANSVERSE SECTIONS

SCALE: 1"=5' VERT.
1"=25' HORIZ.



SCALE:
1"=5' VERT.
1"=25' HORIZ.



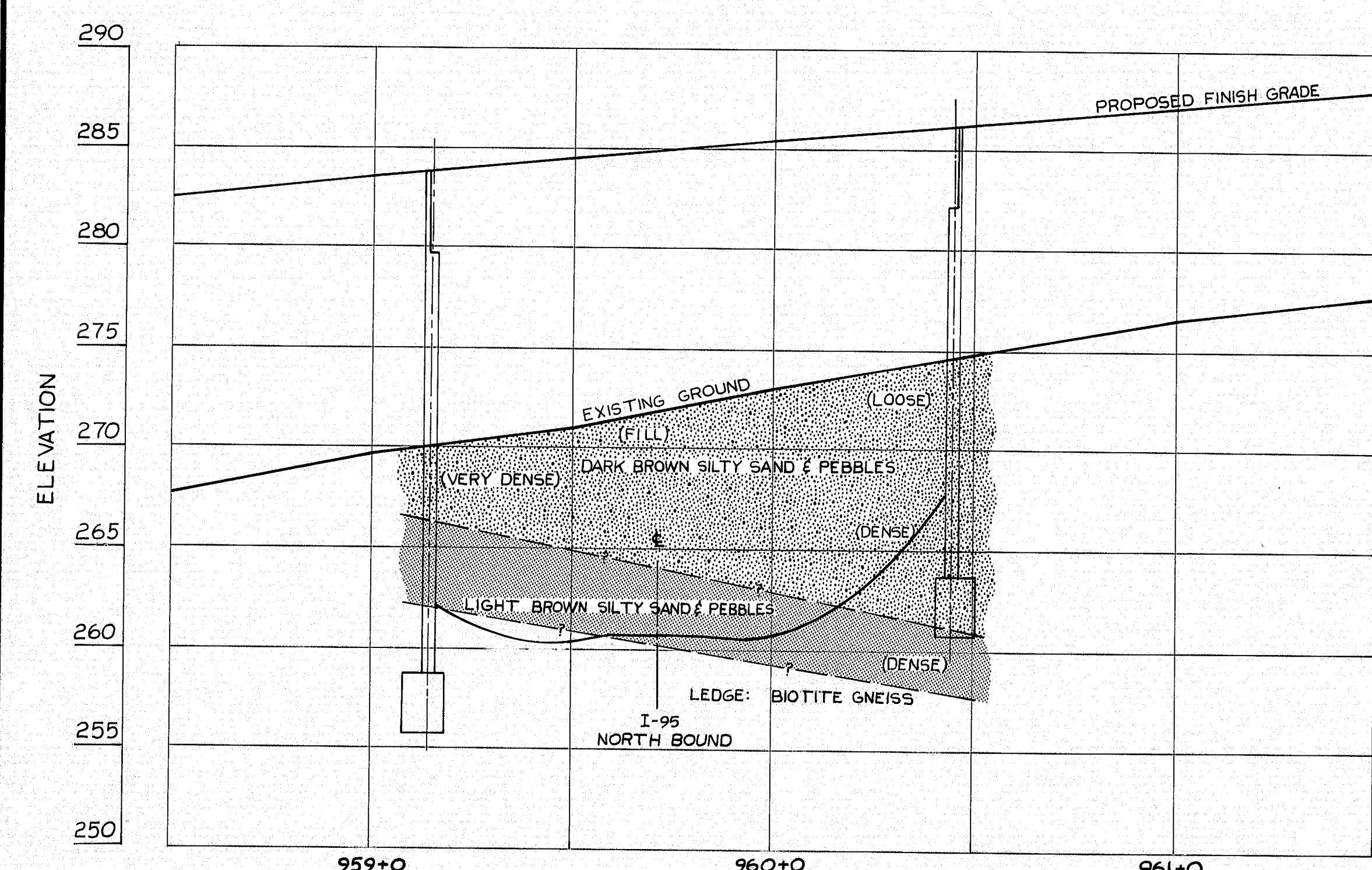
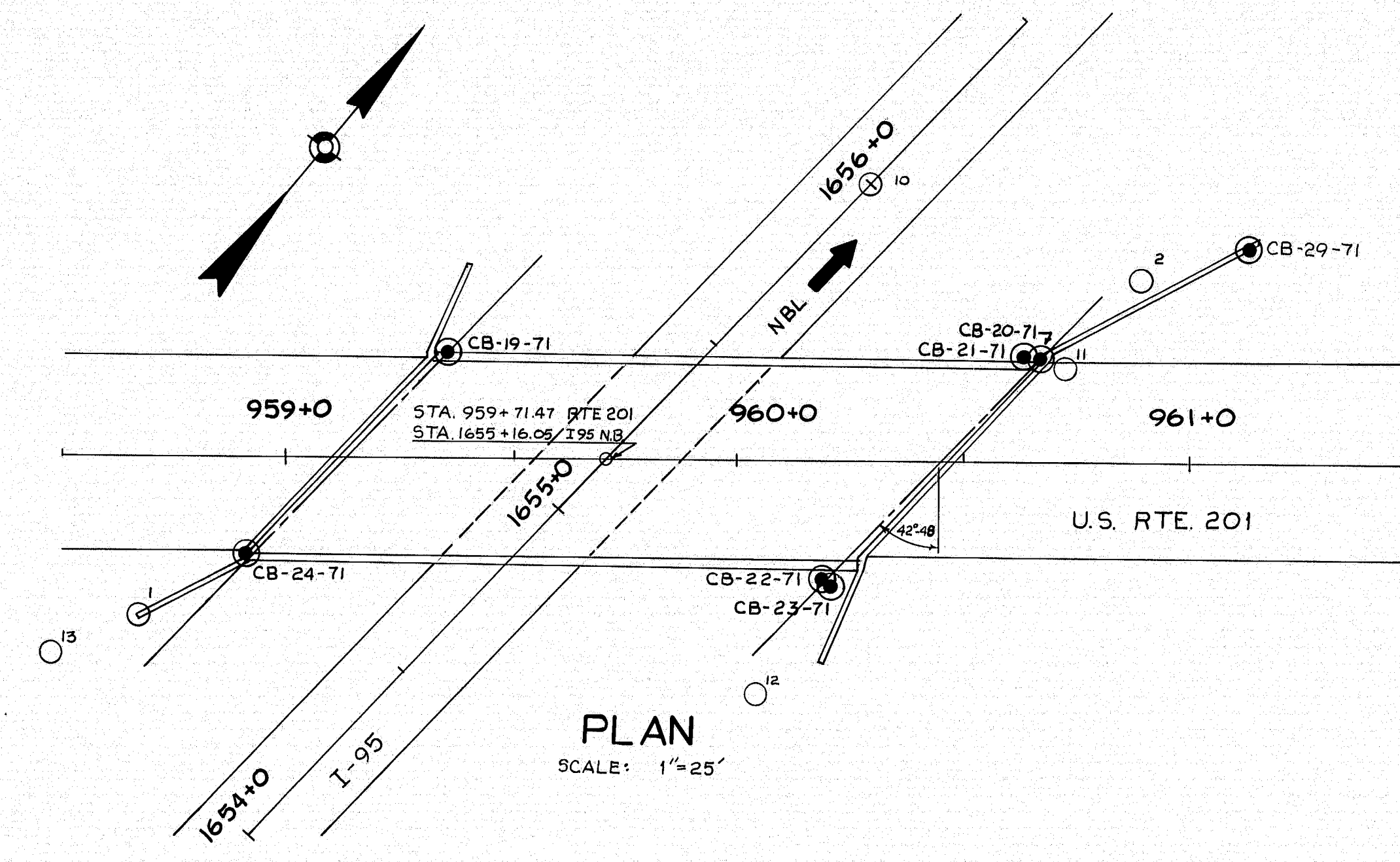
BORING NOTES

- ALL SAMPLES ARE MADE AHEAD OF CASING
- CASING SIZE: 2 1/2"
- NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW
- LOCATION OF SAMPLE OR SAMPLE ATTEMPT
- S & H SAMPLER "J280'S"
- UNSUCCESSFUL SAMPLER ATTEMPT AND TYPE OF SAMPLER
- NUMBER OF BLOWS REQUIRED TO DRIVE SPAD ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW
- BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)
- REFUSAL OF DRILL RODS OR CASING (MAY NOT BE LEDGE)
- LOCATIONS CORDED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
FOUNDATION SURVEY
SHEET 7 OF 55 AUGUSTA, MAINE

152-38

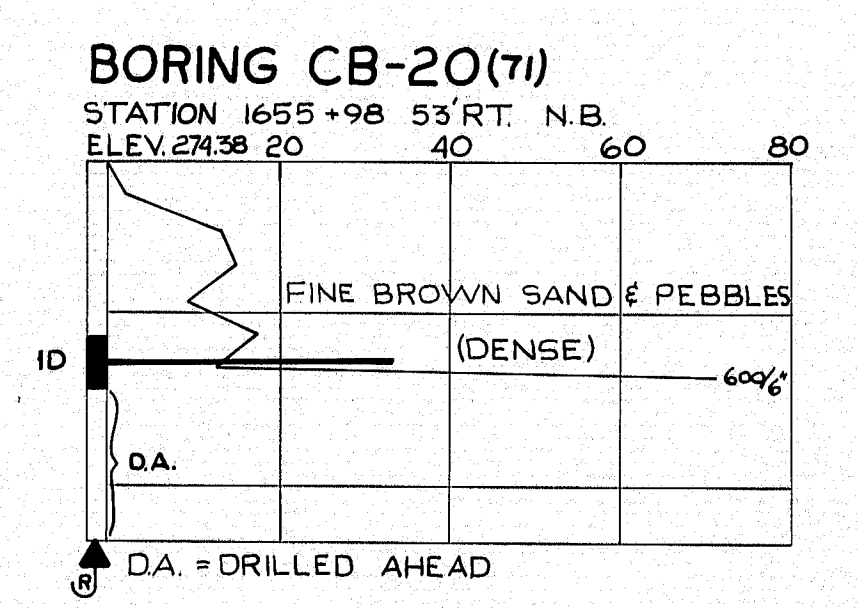
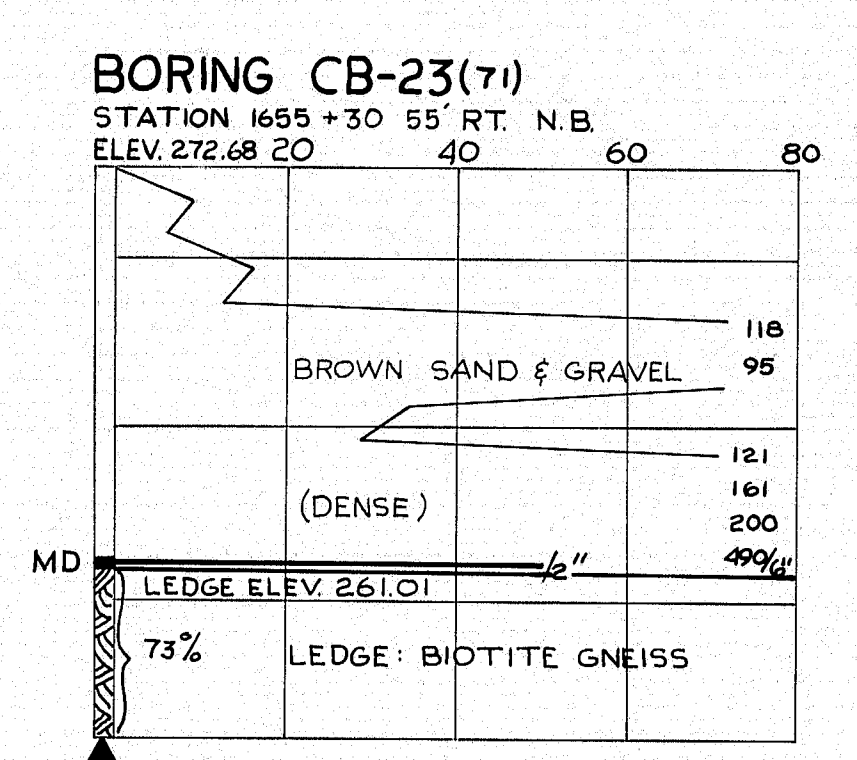
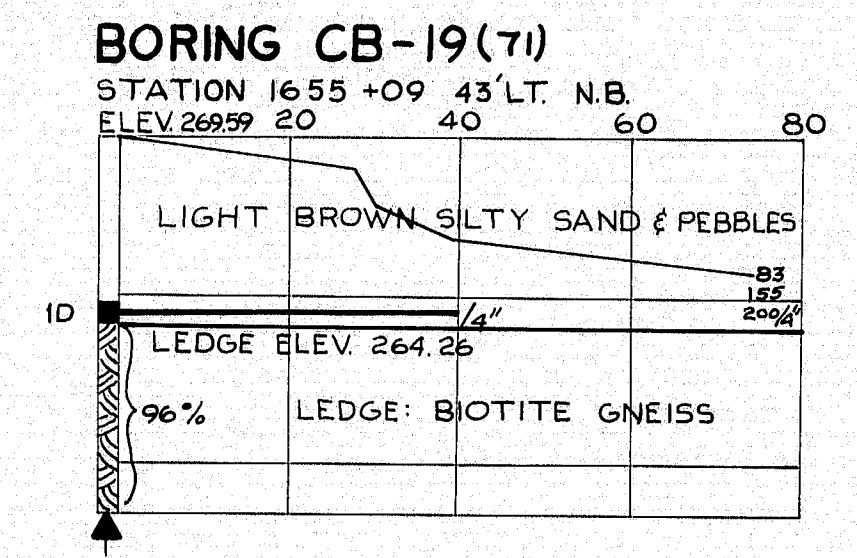
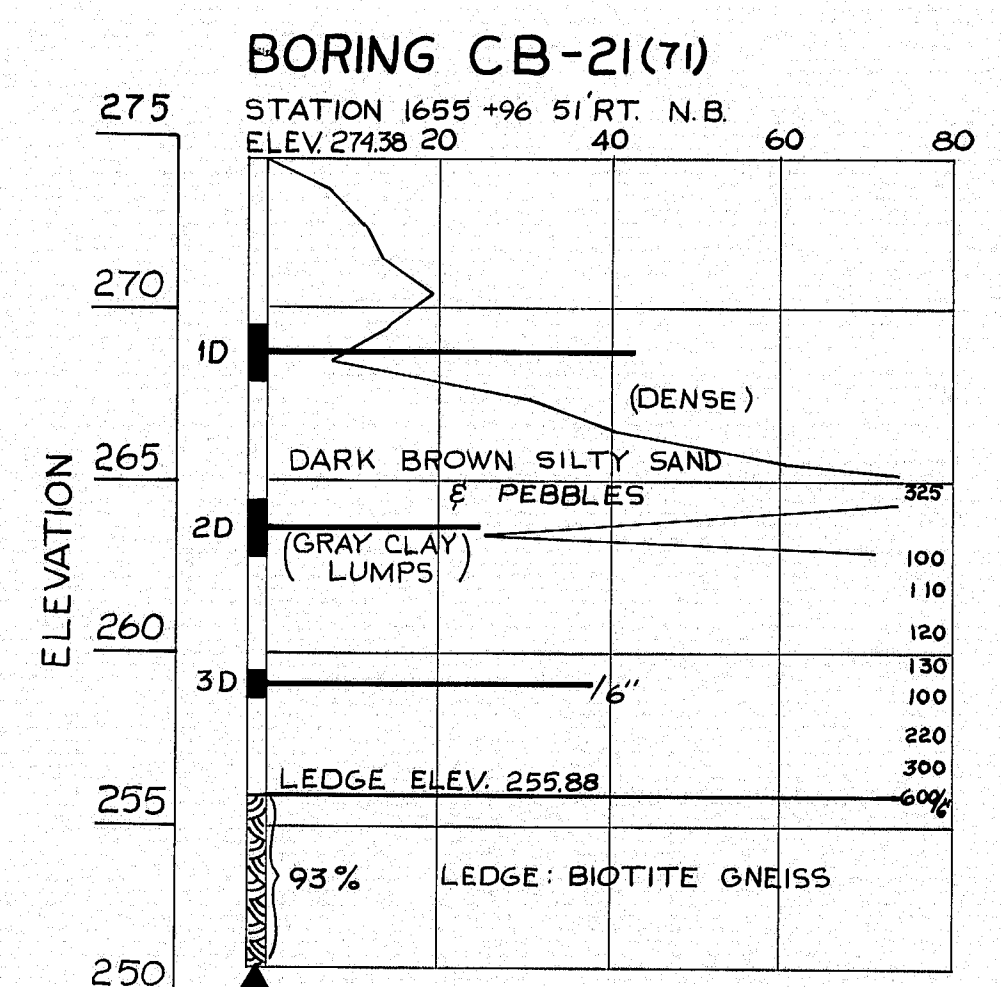
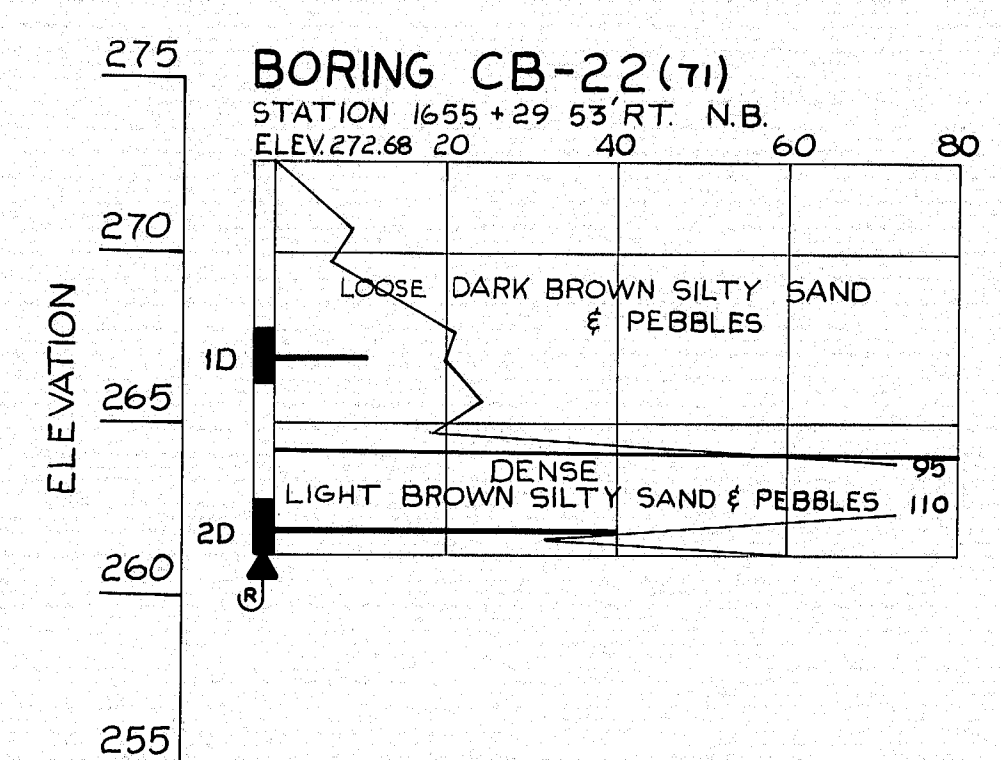
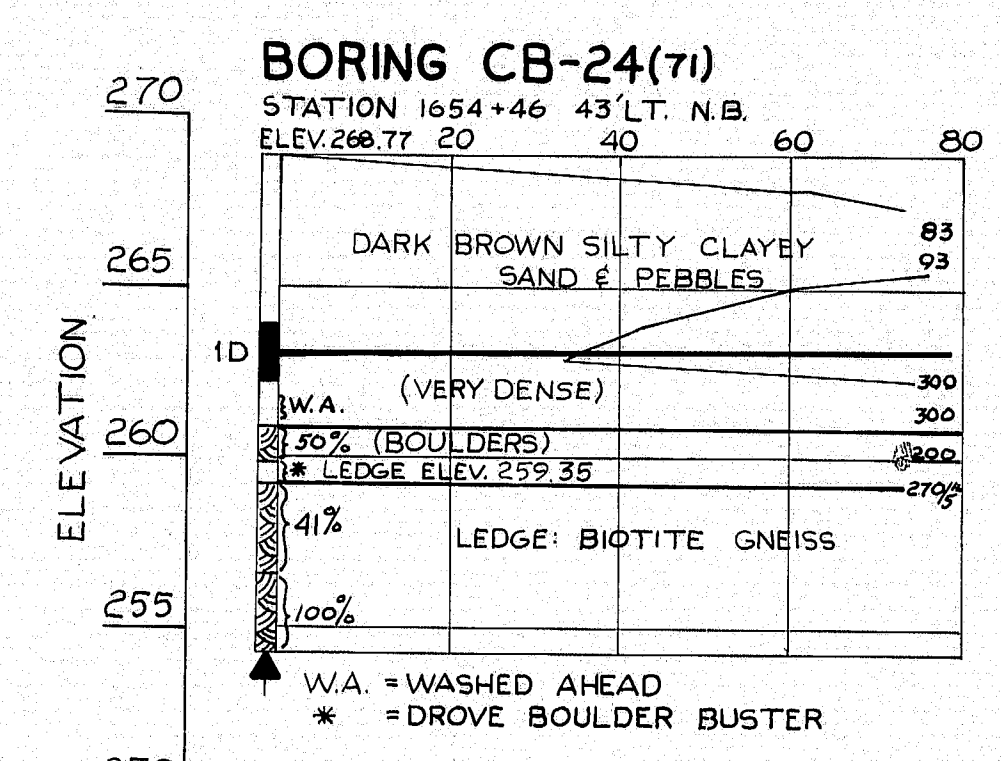
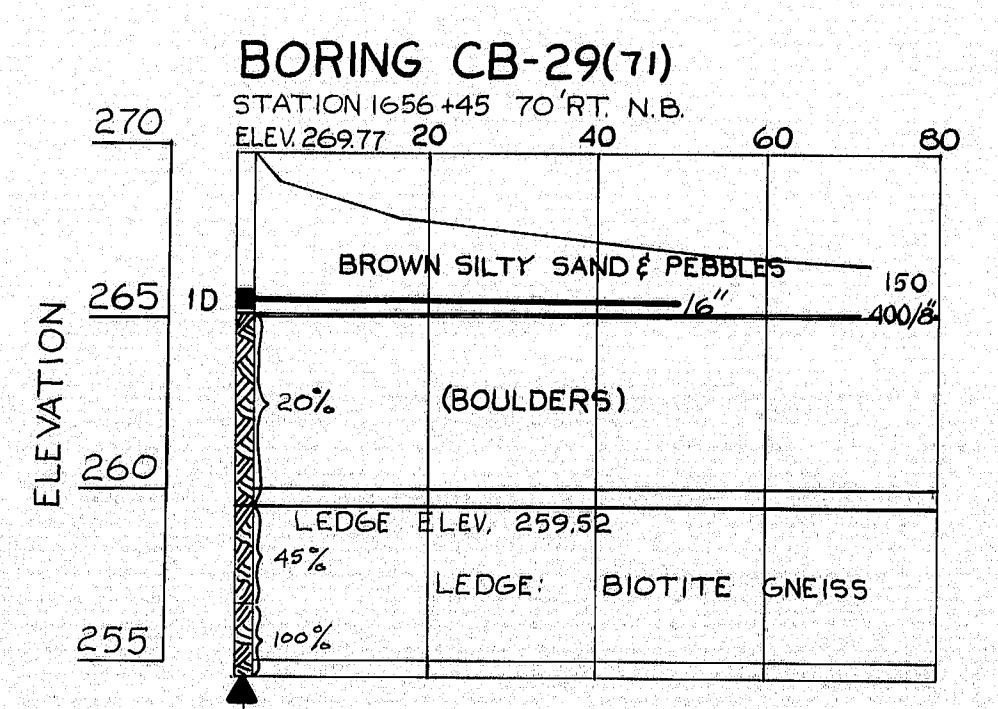
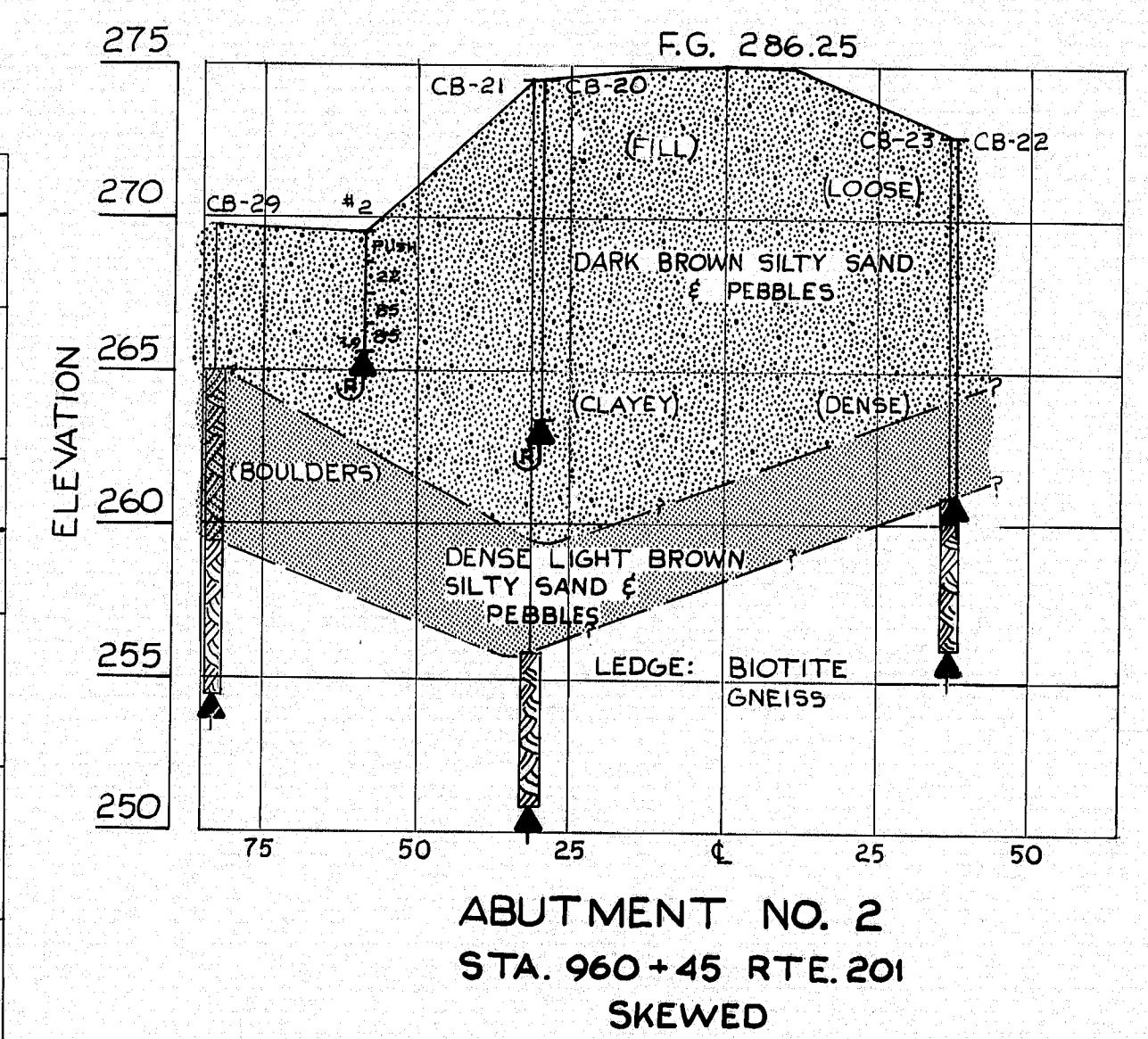
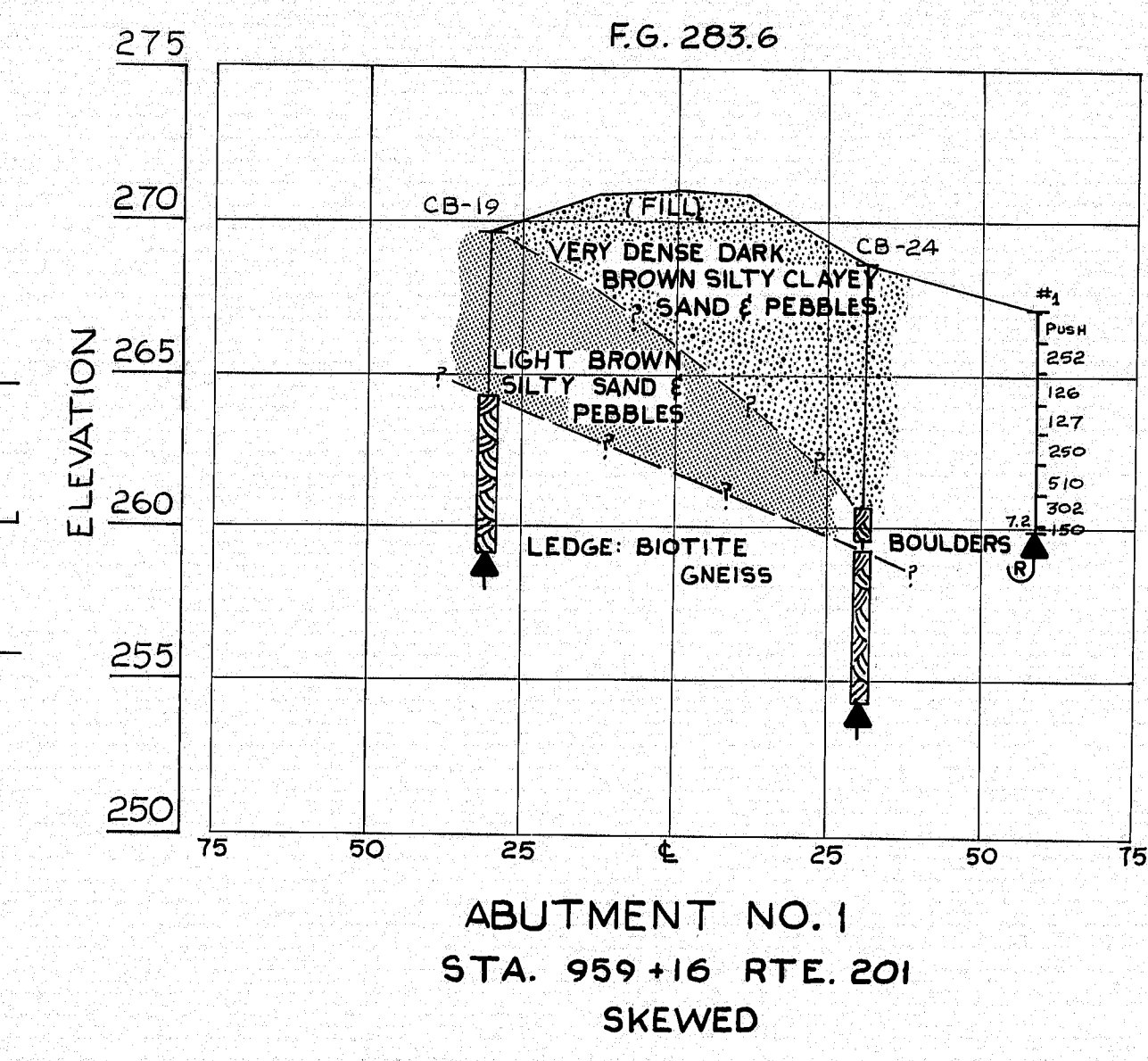
S. F. R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	295 5 (A) 94	28	55



PLANS	DESIGN-DETAILED	CHECKED	REVISIONS	DATE
1/1	1/1	1/1	1/1	3/1/74

TRANSVERSE SECTIONS

SCALE: 1"=5' VERT.
1"=25' HORIZ.



STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
FOUNDATION SURVEY
SHEET 28 OF 55 AUGUSTA, MAINE

152-39

S. P. R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I 85-5(6) 94	9	55

Notes:

All surfaces so designated on the plans shall be sandblasted. These surfaces shall be carried to a minimum depth of 18" below the finished ground.

Special care shall be exercised so that form joints at the exposed face of concrete shall be tight.

Before sandblasting, all fins and projections in the concrete shall be removed and all holes patched to create a surface of uniform texture.

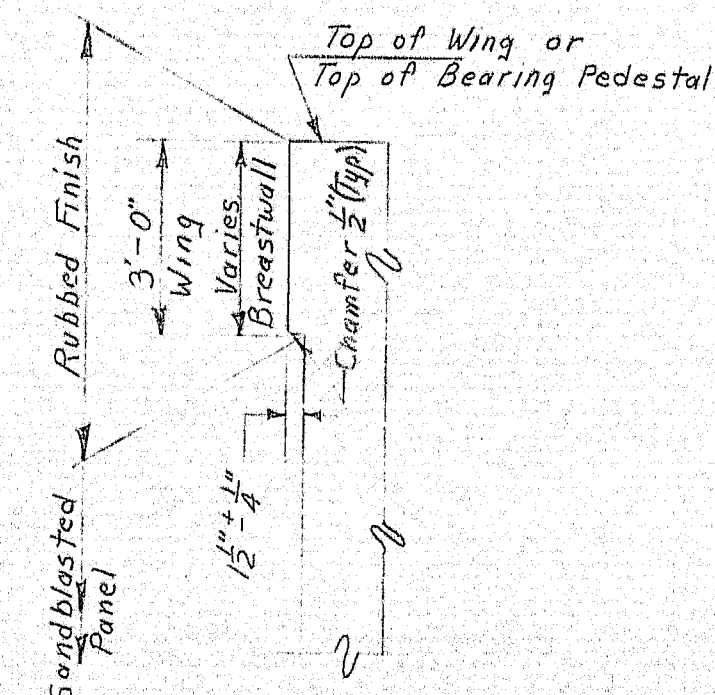
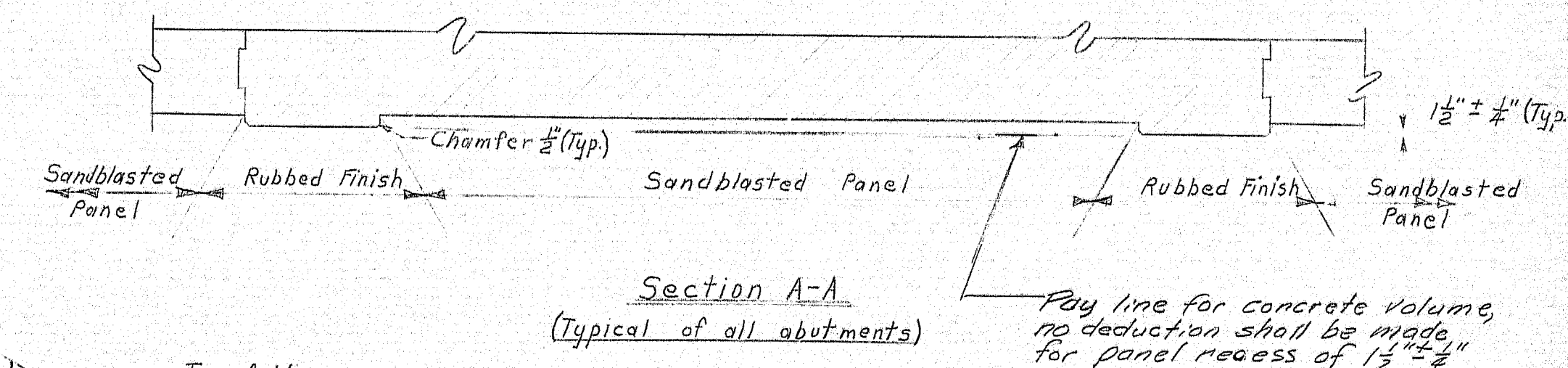
In order to insure a consistent surface texture for the areas to be architecturally treated, concrete aggregate shall be from the same source and portland cement shall be from the same manufacturer throughout the entire placement of the abutment wings and breastwall.

At the time the concrete is placed, the contractor shall cast 3 sample slabs (2'x2'x4"). Prior to sandblasting, the samples shall be sandblasted, each a different degree of penetration with a maximum depth of $\frac{1}{8}$ " of an inch, approximately, and under the direction of the Engineer. The most desirable sample will be chosen by the Engineer, and the designated areas shall be sandblasted to match this sample.

Concrete shall not be sandblasted for at least 28 days after placement.

The contractor shall take all the necessary steps to protect materials and equipment from damage by the sandblasting operation. Personnel shall be properly equipped: sandblast hood for operator, and respirators and goggles for all other personnel exposed to dust.

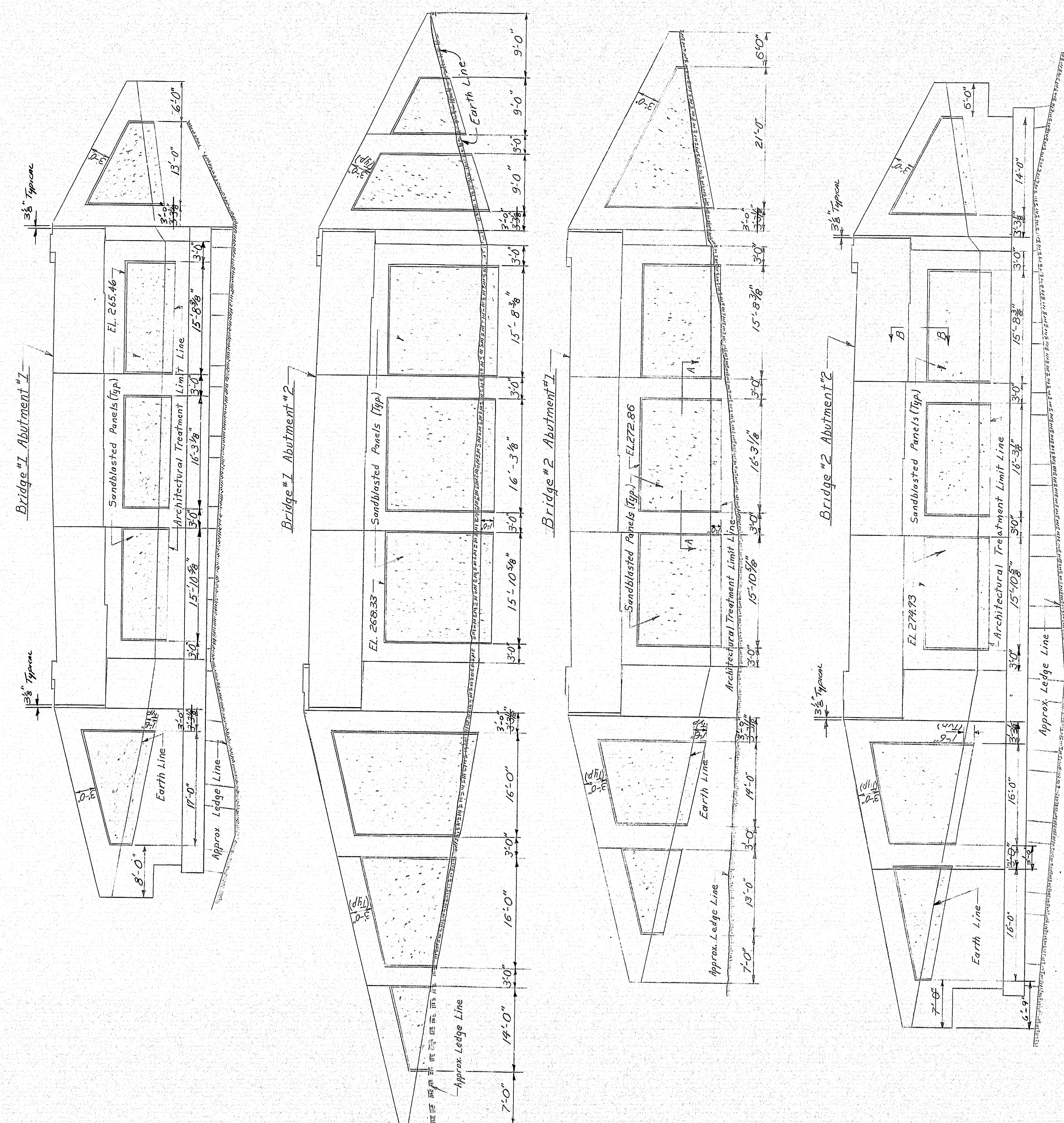
Payment for the sandblasting shall be included in the contract unit price for Item 502.21, "Structural Concrete, Abutments and Retaining Walls".



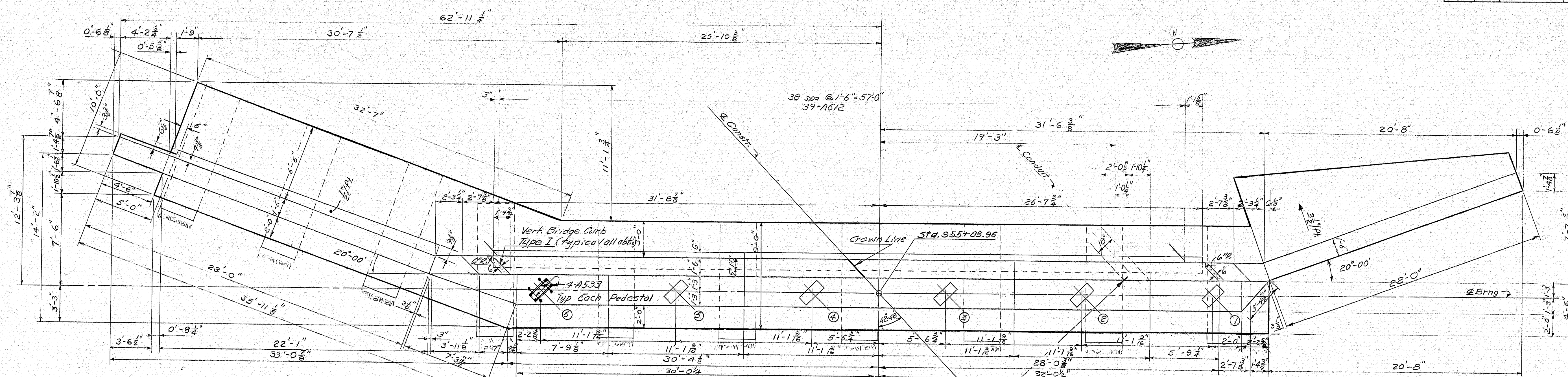
STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
ARCHITECTURAL TREATMENT
SHEET 9 OF 55 AUGUSTA, MAINE APRIL 1971

152-40

PLANS	DESIGN - DETAIL	BY	DATE
DESIGNED	ERC	WJZ	3.18.71
CHECKED	ERC		
REVISIONS			
FIELD CHANGES		WJZ	3/15/71



S. P. N.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195 5(6)94	10	53

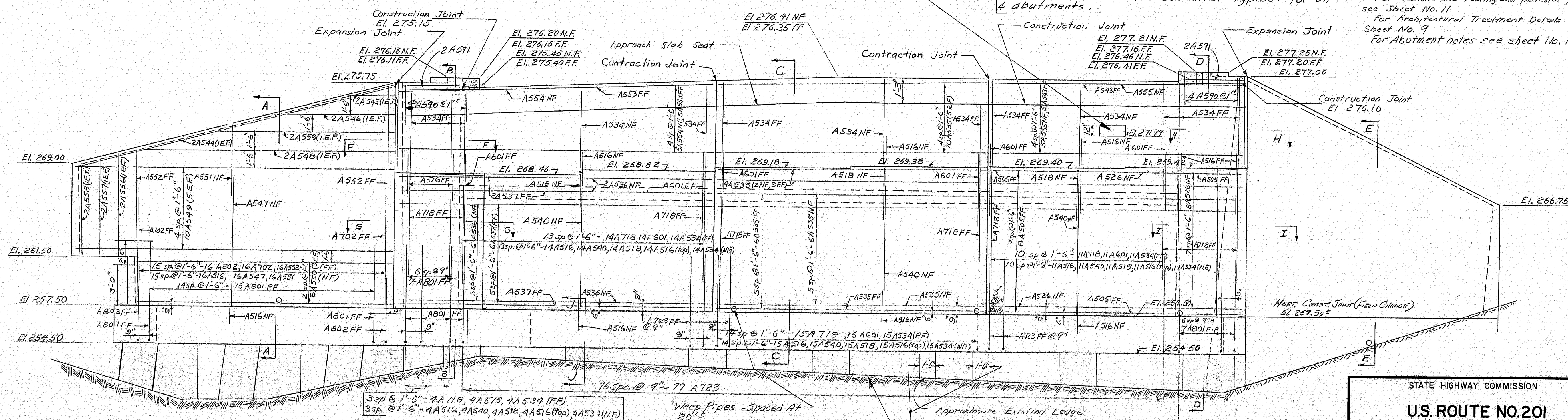


Note: See sheet No. 26 for details of Vertical Bridge Curb Type I

PLAN

Note: The Contractor shall cast concrete around the Telephone Conduits as they are placed or after they are placed by the Utility Co. The conduits are to be placed after the Superstructure Structural Slab is placed and cured. Class A Concrete or Mortar in conformance with Section 705 may be used for this purpose. The cost of casting this Concrete or Mortar is to be paid for under Item 502.21 with no deduction being made for the volume of the conduits. Typical for all 4 abutments.

Sheet Cross References:
 For Sections and Footing and pedestal plans see Sheet No. 11
 For Architectural Treatment Details see Sheet No. 9
 For Abutment notes see sheet No. 12.

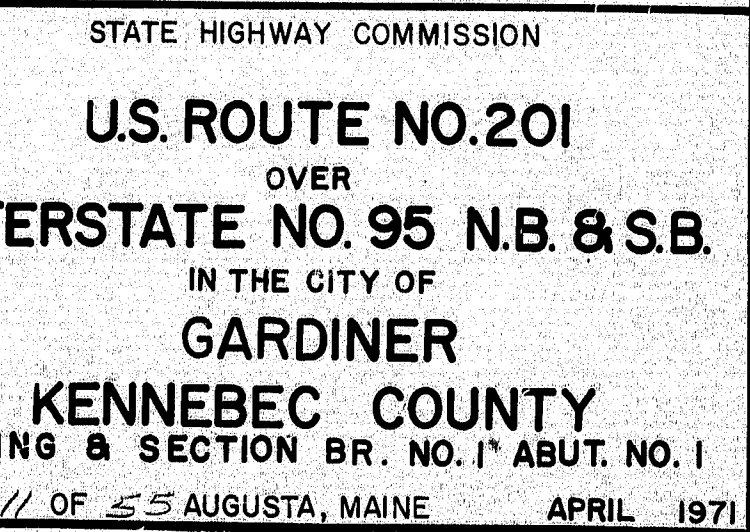
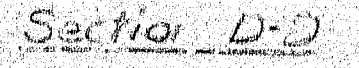
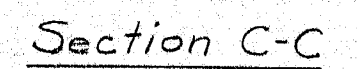
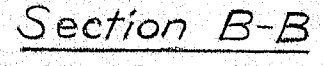
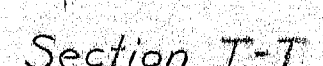
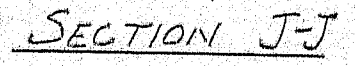
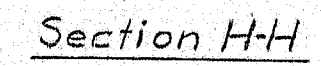
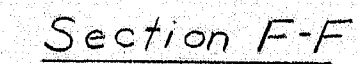
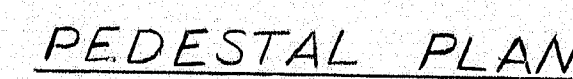


ELEVATION

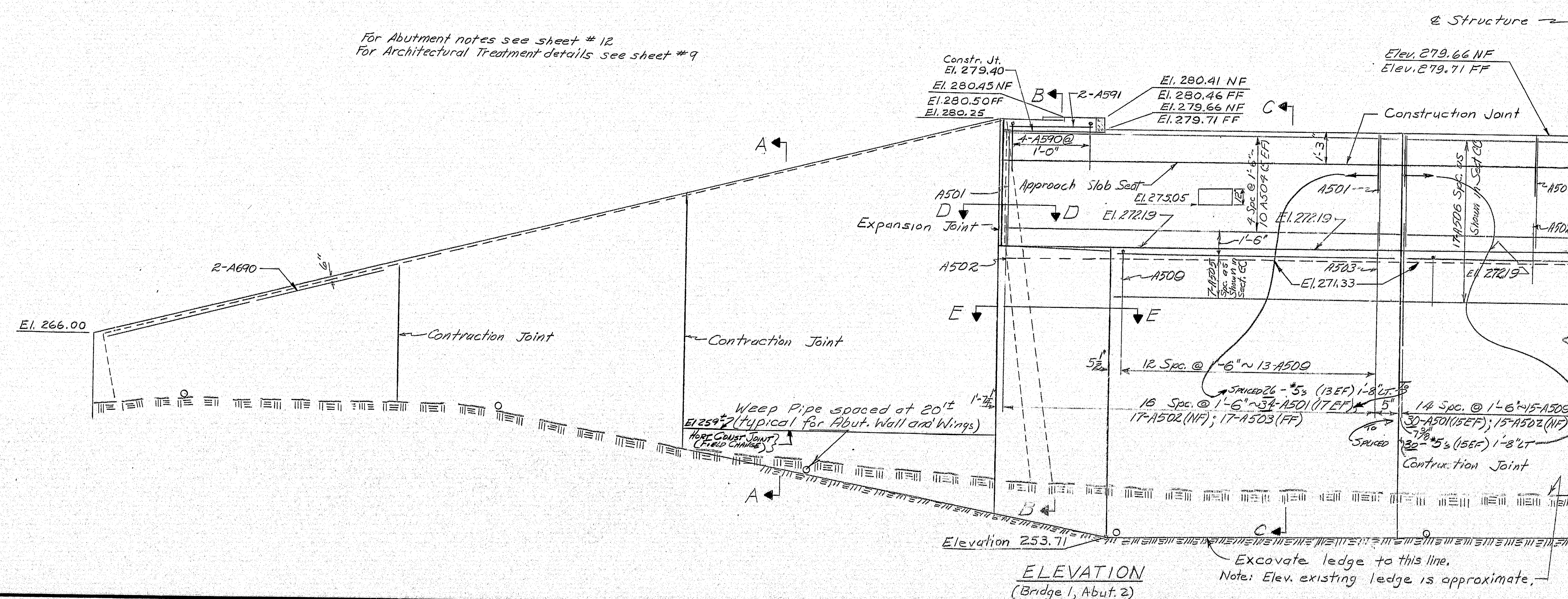
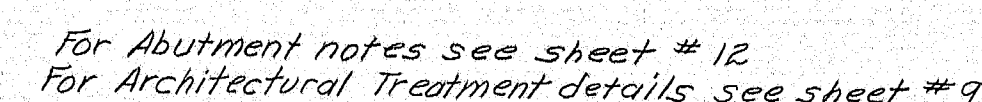
DESIGNED BY	DATE
CHECKED BY	DATE
REVISIONS	
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STATE HIGHWAY COMMISSION
 U.S. ROUTE NO. 201
 OVER
 INTERSTATE NO. 95 N.B. & S.B.
 IN THE CITY OF
 GARDNER
 KENNEBEC COUNTY
 BRIDGE NO. 1 ABUTMENT NO. 1
 SHEET 10 OF 53 AUGUSTA, MAINE APRIL 1971

152-41



PLANS <i>As Built</i>	<i>Proj. Des. Exp. All.</i>		BY	DATE
	DESIGN - DETAILED		<i>C.R.</i>	<i>11/2/83</i>
	CHECKED		<i>EBC</i>	
	REVISIONS			
	FIELD CHANGES		<i>WCE</i>	<i>3/5/90</i>



10-11-12-13-14-15-16-17-18-19-20-21-22-23-24-25-26-27-28-29-30-31-32-33-34-35-36-37-38-39-40-41-42-43-44-45-46-47-48-49-50-51-52-53-54-55-56-57-58-59-60-61-62-63-64-65-66-67-68-69-70-71-72-73-74-75-76-77-78-79-80-81-82-83-84-85-86-87-88-89-90-91-92-93-94-95-96-97-98-99-100-101-102-103-104-105-106-107-108-109-110-111-112-113-114-115-116-117-118-119-120-121-122-123-124-125-126-127-128-129-130-131-132-133-134-135-136-137-138-139-140-141-142-143-144-145-146-147-148-149-150-151-152-153-154-155-156-157-158-159-160-161-162-163-164-165-166-167-168-169-170-171-172-173-174-175-176-177-178-179-180-181-182-183-184-185-186-187-188-189-190-191-192-193-194-195-196-197-198-199-200-201-202-203-204-205-206-207-208-209-210-211-212-213-214-215-216-217-218-219-220-221-222-223-224-225-226-227-228-229-230-231-232-233-234-235-236-237-238-239-240-241-242-243-244-245-246-247-248-249-250-251-252-253-254-255-256-257-258-259-260-261-262-263-264-265-266-267-268-269-270-271-272-273-274-275-276-277-278-279-280-281-282-283-284-285-286-287-288-289-290-291-292-293-294-295-296-297-298-299-300-301-302-303-304-305-306-307-308-309-310-311-312-313-314-315-316-317-318-319-320-321-322-323-324-325-326-327-328-329-330-331-332-333-334-335-336-337-338-339-340-341-342-343-344-345-346-347-348-349-350-351-352-353-354-355-356-357-358-359-360-361-362-363-364-365-366-367-368-369-370-371-372-373-374-375-376-377-378-379-380-381-382-383-384-385-386-387-388-389-390-391-392-393-394-395-396-397-398-399-400-401-402-403-404-405-406-407-408-409-410-411-412-413-414-415-416-417-418-419-420-421-422-423-424-425-426-427-428-429-430-431-432-433-434-435-436-437-438-439-440-441-442-443-444-445-446-447-448-449-450-451-452-453-454-455-456-457-458-459-460-461-462-463-464-465-466-467-468-469-470-471-472-473-474-475-476-477-478-479-480-481-482-483-484-485-486-487-488-489-490-491-492-493-494-495-496-497-498-499-500-501-502-503-504-505-506-507-508-509-510-511-512-513-514-515-516-517-518-519-520-521-522-523-524-525-526-527-528-529-530-531-532-533-534-535-536-537-538-539-540-541-542-543-544-545-546-547-548-549-550-551-552-553-554-555-556-557-558-559-560-561-562-563-564-565-566-567-568-569-570-571-572-573-574-575-576-577-578-579-580-581-582-583-584-585-586-587-588-589-590-591-592-593-594-595-596-597-598-599-600-601-602-603-604-605-606-607-608-609-610-611-612-613-614-615-616-617-618-619-620-621-622-623-624-625-626-627-628-629-630-631-632-633-634-635-636-637-638-639-640-641-642-643-644-645-646-647-648-649-650-651-652-653-654-655-656-657-658-659-660-661-662-663-664-665-666-667-668-669-670-671-672-673-674-675-676-677-678-679-680-681-682-683-684-685-686-687-688-689-690-691-692-693-694-695-696-697-698-699-700-701-702-703-704-705-706-707-708-709-710-711-712-713-714-715-716-717-718-719-720-721-722-723-724-725-726-727-728-729-730-731-732-733-734-735-736-737-738-739-740-741-742-743-744-745-746-747-748-749-750-751-752-753-754-755-756-757-758-759-760-761-762-763-764-765-766-767-768-769-770-771-772-773-774-775-776-777-778-779-780-781-782-783-784-785-786-787-788-789-790-791-792-793-794-795-796-797-798-799-800-801-802-803-804-805-806-807-808-809-810-811-812-813-814-815-816-817-818-819-820-821-822-823-824-825-826-827-828-829-830-831-832-833-834-835-836-837-838-839-840-841-842-843-844-845-846-847-848-849-850-851-852-853-854-855-856-857-858-859-860-861-862-863-864-865-866-867-868-869-870-871-872-873-874-875-876-877-878-879-880-881-882-883-884-885-886-887-888-889-890-891-892-893-894-895-896-897-898-899-900-901-902-903-904-905-906-907-908-909-910-911-912-913-914-915-916-917-918-919-920-921-922-923-924-925-926-927-928-929-930-931-932-933-934-935-936-937-938-939-940-941-942-943-944-945-946-947-948-949-950-951-952-953-954-955-956-957-958-959-960-961-962-963-964-965-966-967-968-969-970-971-972-973-974-975-976-977-978-979-980-981-982-983-984-985-986-987-988-989-990-991-992-993-994-995-996-997-998-999-1000-1001-1002-1003-1004-1005-1006-1007-1008-1009-1010-1011-1012-1013-1014-1015-1016-1017-1018-1019-1020-1021-1022-1023-1024-1025-1026-1027-1028-1029-1030-1031-1032-1033-1034-1035-1036-1037-1038-1039-1040-1041-1042-1043-1044

STATE HIGHWAY COMMISSION

U.S. ROUTE NO.201

OVER

INTERSTATE NO.95 N.B.&S.B

IN THE CITY OF

GARDINER

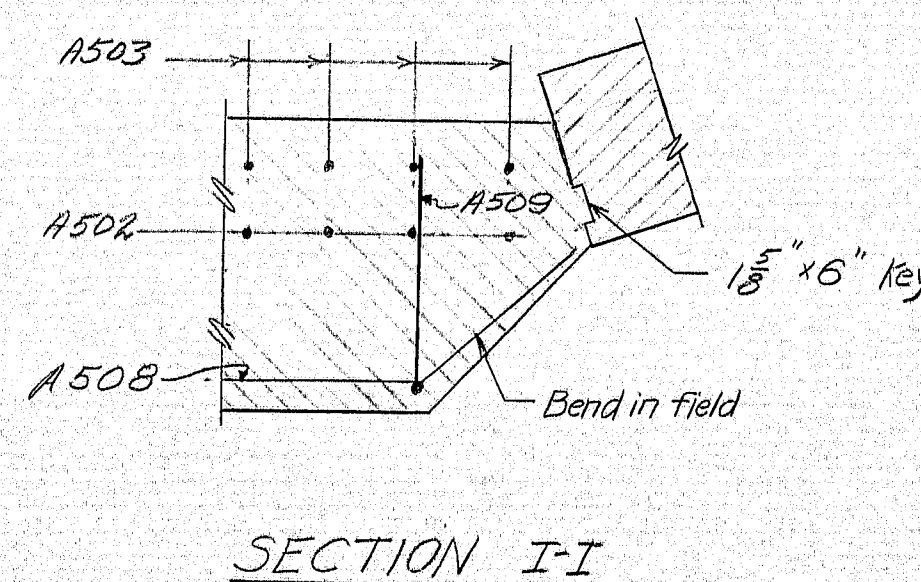
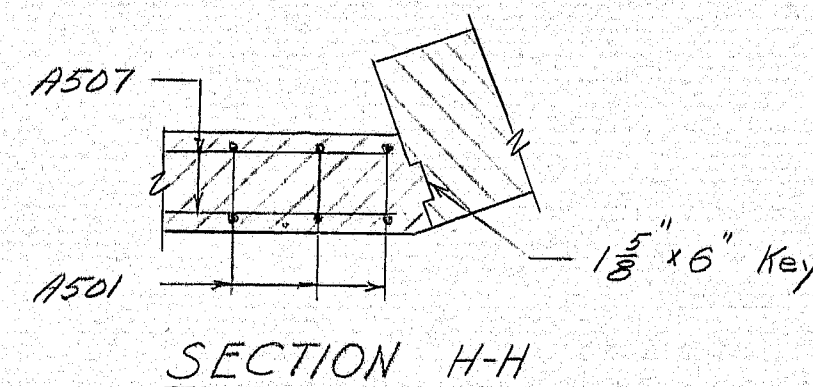
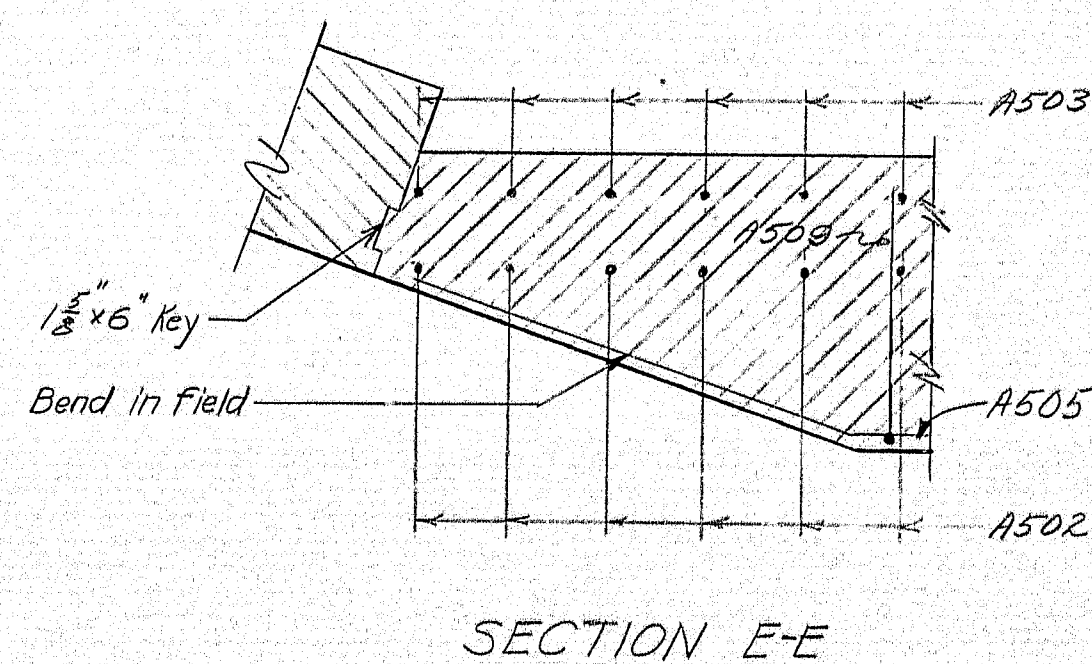
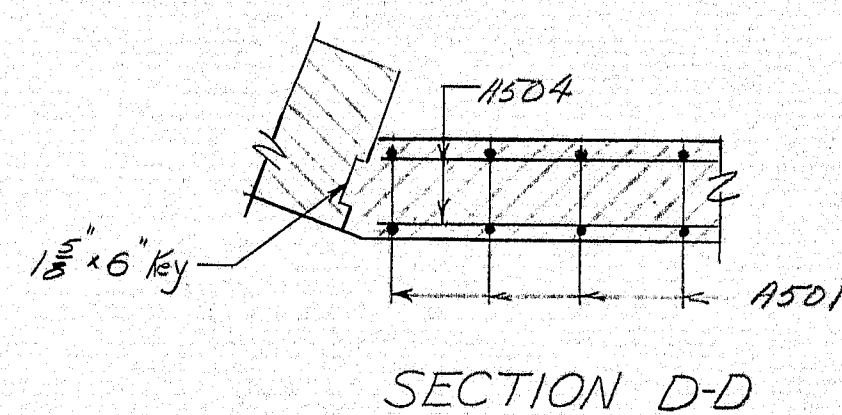
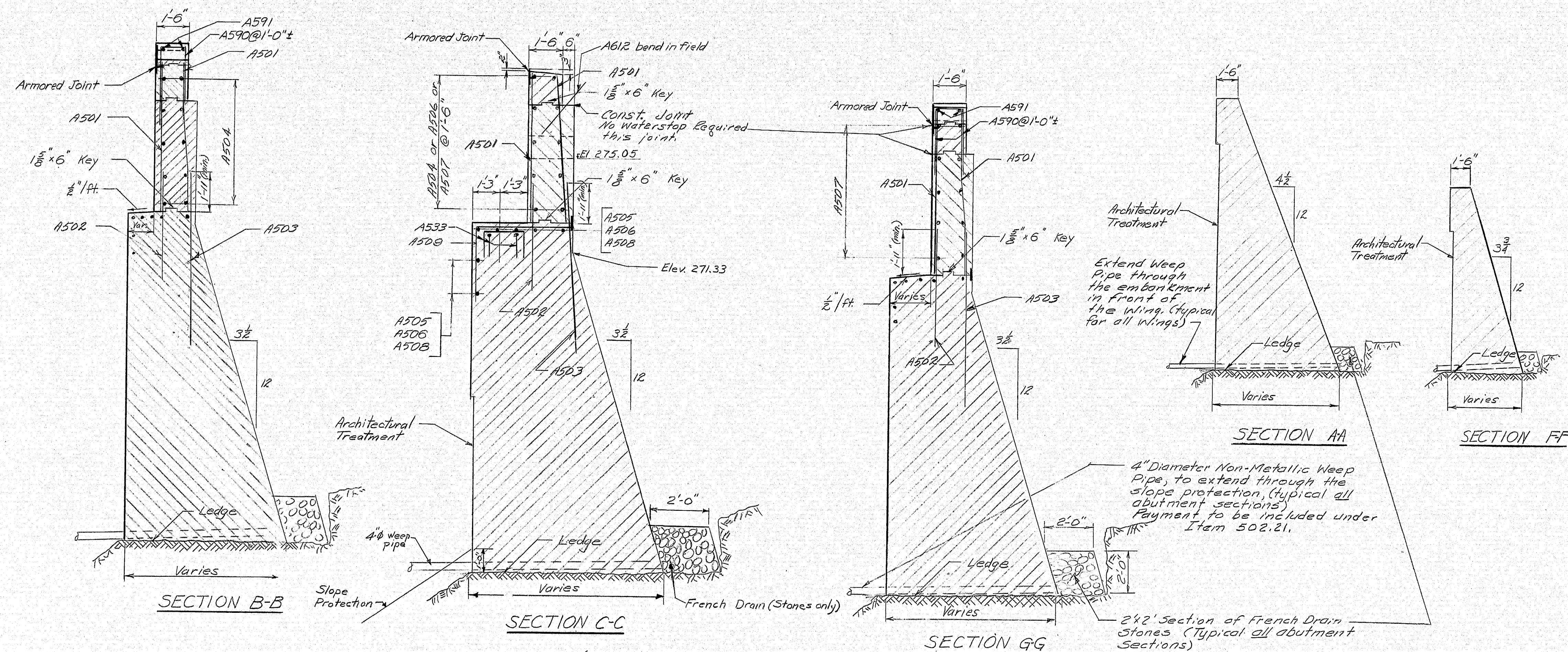
KENNEBEC COUNTY

BRIDGE NO. 1 ABUTMENT NO. 2

SHEET 13 OF 55 AUGUSTA, MAINE APRIL 1968

152-44

S.P.R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195-57694	14	55



Note:
For Architectural Treatment details see sheet #9
For Abutment Notes see sheet #2

Max. Toe Pressures		
Abut. #1	Bridge #1	19.4 k/s.f.
Abut. #2	Bridge #1	20.7 k/s.f.
Abut. #1	Bridge #2	25.0 k/s.f.
Abut. #2	Bridge #2	22.9 k/s.f.

DATE	BY	REVISIONS
3/17/74	CR	DESIGN - DETAILED
	EBC	CHECKED
	WCE	REVISIONS
		FIELD CHANGES

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
SECTIONS BR. NO. 1 ABUT. NO. 2
SHEET 14 OF 55 AUGUSTA, MAINE APRIL 1971

152-45

PLANS	By: <i>Dr. Eng. Ali</i>	BY	DATE
	DESIGN - DETAILED	<i>C.R.</i>	<i>J.R.B.</i>
	CHECKED	<i>EBC</i>	
	REVISIONS		
	FIELD CHANGES	<i>none</i>	<i>1-1-77</i>

Part PlanPart Elevation

STATE HIGHWAY COMMISSION

U.S. ROUTE NO. 201

OVER

INTERSTATE NO. 95 N.B. & S.B.

IN THE CITY OF

GARDINER

KENNEBEC COUNTY

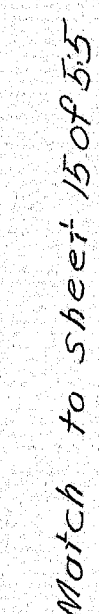
BRIDGE NO. 2 ABUTMENT NO. 1

SHEET 15 OF 55 AUGUSTA, MAINE APRIL 1971

152-46

[illegible]

For Abutment Notes see sheet #12
For Arch. Treatment Details see sheet #9



PLANS <i>As Built</i>	<i>Proj. No. Exp. All.</i>		BY	DATE
	DESIGN - DETAILED		<i>C.R.</i>	<i>J.R.B.</i>
	CHECKED		<i>EBC</i>	
	REVISIONS			
	FIELD CHANGES		<i>WCF</i>	<i>3/1/74</i>

STATE HIGHWAY COMMISSION

U.S. ROUTE NO.201

OVER

INTERSTATE NO. 95 N.B. & S.B.

IN THE CITY OF

GARDINER

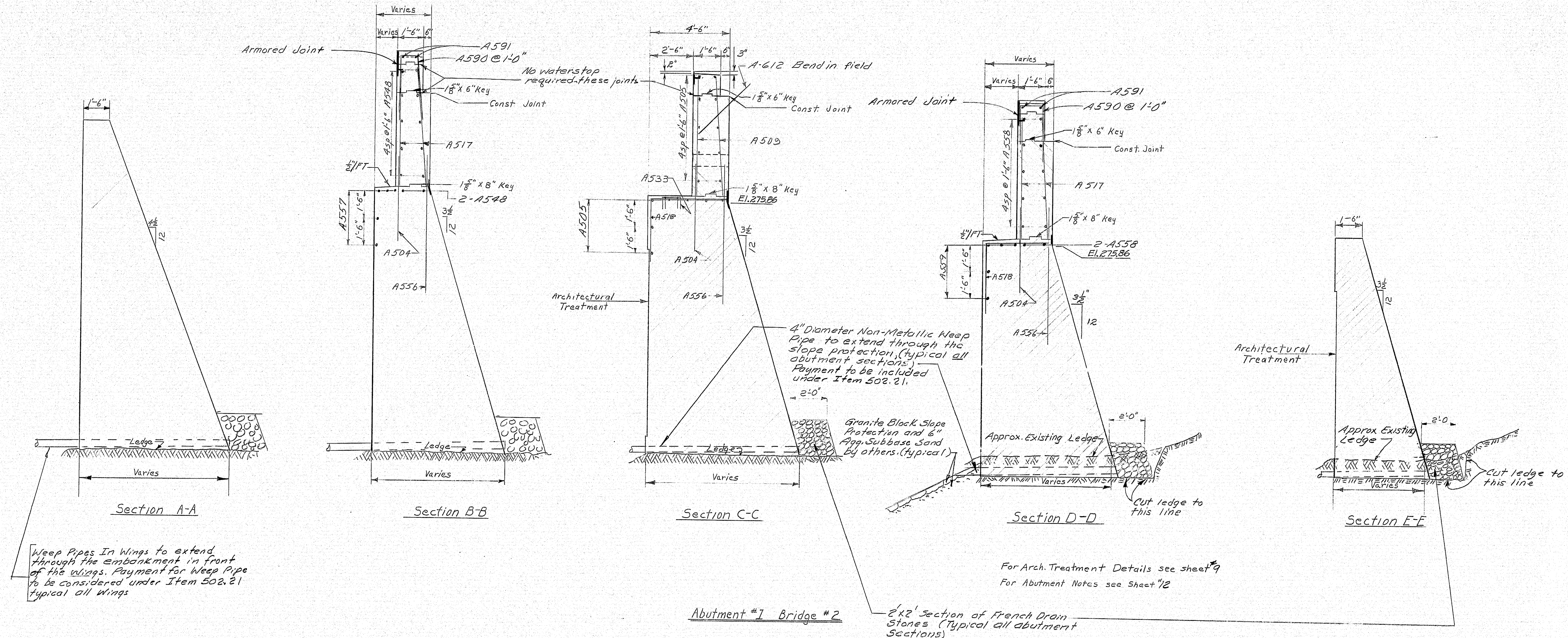
KENNEBEC COUNTY

BRIDGE NO. 2 ABUTMENT NO. 1

SHEET 16 OF 55 AUGUSTA, MAINE APRIL 1971

152-47

S. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195 5(6)94	17	55



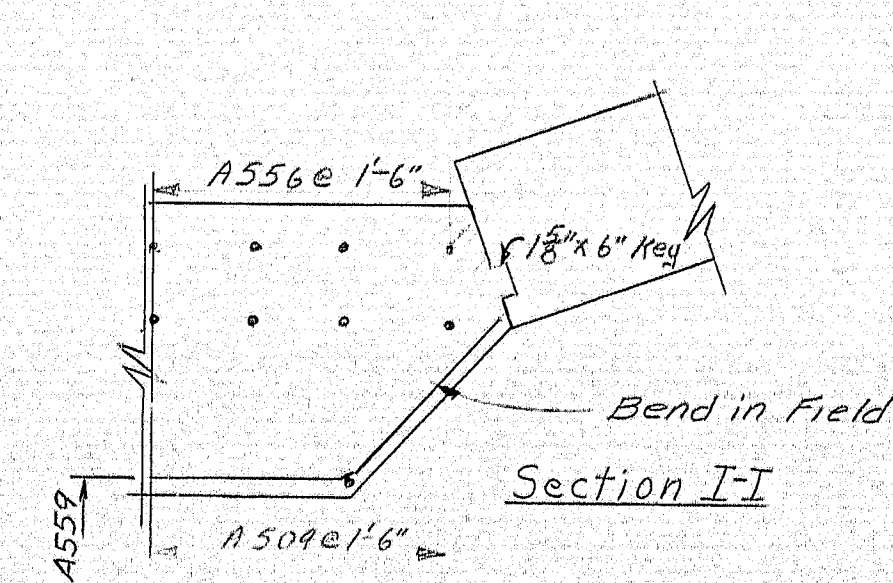
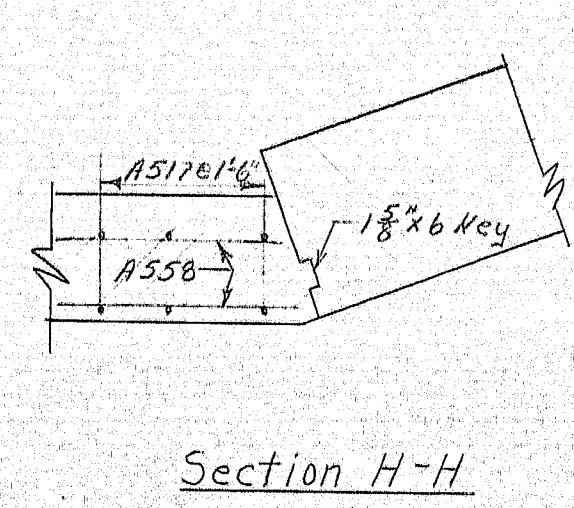
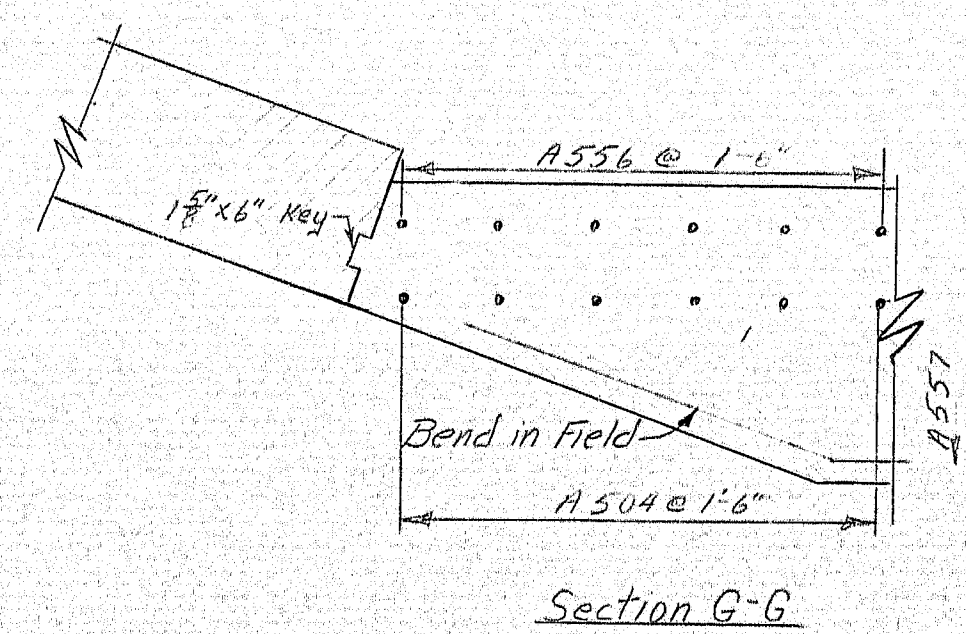
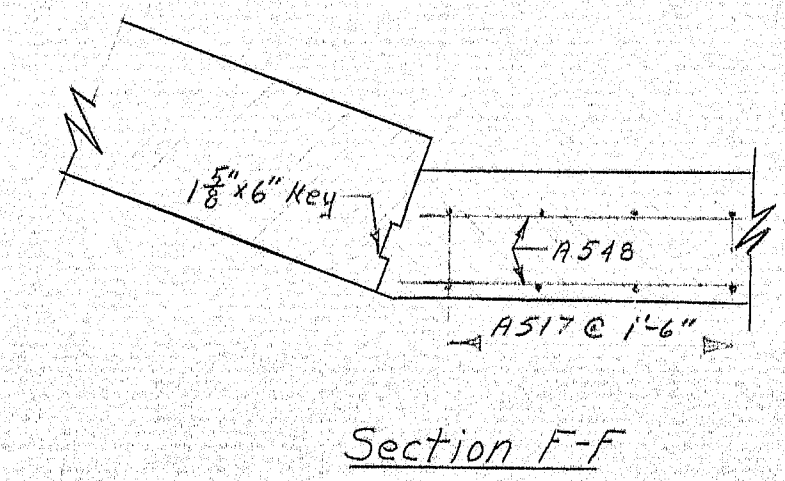
Keep Pipes In Wings to extend through the embankment in front of the wings. Payment for Weep Pipe to be considered under Item 502.21 typical all wings

For Arch. Treatment Details see sheet 9
For Abutment Notes see sheet 12

Abutment #1 Bridge #2

2' x 2' Section of French Drain Stones (typical all abutment sections)

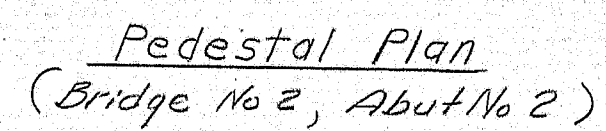
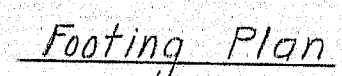
DATE	BY
3/9/74	J.R.B.
DESIGN - DETAILED	CHECKED
	EBC
REVISIONS	FIELD CHANGES
1/2	WLC



STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
SECTIONS BR. NO. 2 ABUT. NO. 1
SHEET 17 OF 55 AUGUSTA, MAINE APRIL 1971

152-48

PLANS <i>As Built</i>	<i>Copy Rec. Exp. All</i>		BY		DATE
	DESIGN - DETAILED		C.R.	J.R.B.	
	CHECKED		<i>ERC</i>		
	REVISIONS				
	FIELD CHANGES		<i>NCE</i>		<i>3/5/74</i>



Construction

STATE HIGHWAY COMMISSION

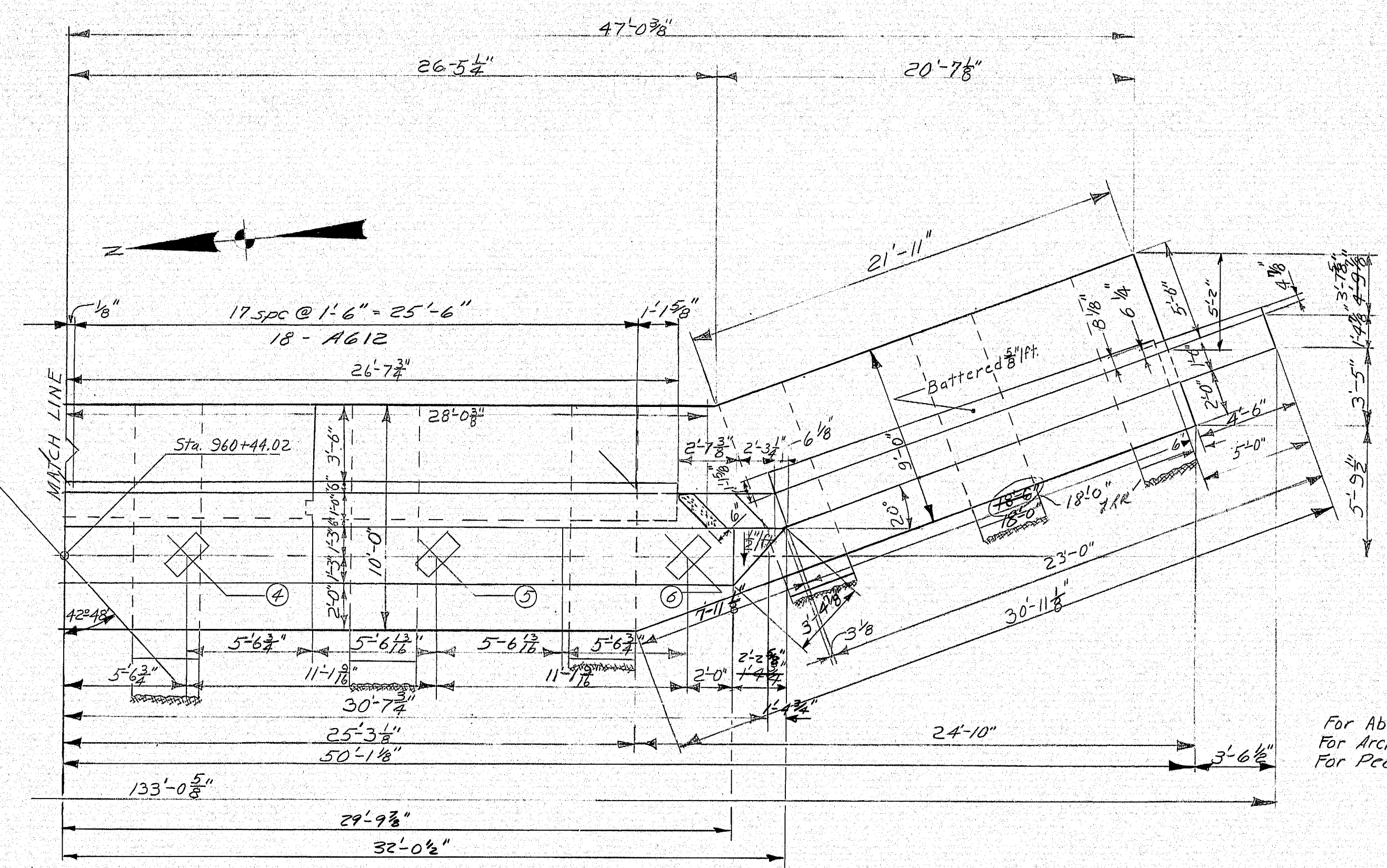
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
FOOTING BR. NO. 2 ABUT. NO. 2

SHEET 12 OF 55 AUGUSTA, MAINE APRIL 1971

SHEET 18 OF 55 AUGUSTA, MAINE APRIL 1971

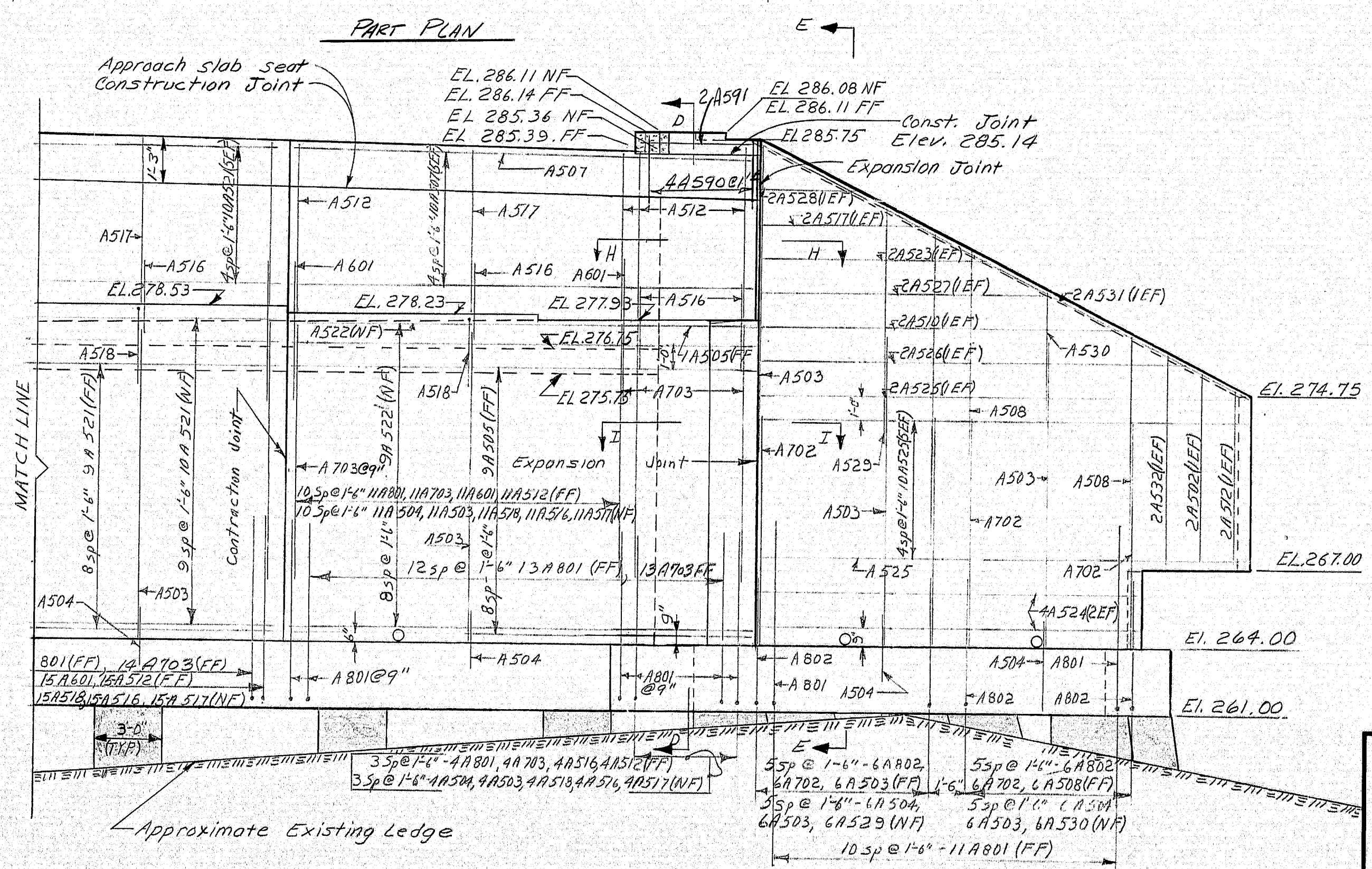
152-49

R. P. R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195 5(6)94	19	55



For Abutment Notes see Sheet #12
For Architectural Treatment see sheet #9
For Pedestal details see sheets #18 & #21

Match to sheet 20 of 55



Part Elevation
Before preparing Abutment Foundation
see Abutment notes sheet 12 of 55

DATE	BY	REVISIONS
3/5/74	J.R.B.	1.0
		2.0
		3.0
		4.0
		5.0
		6.0
		7.0
		8.0
		9.0
		10.0
		11.0
		12.0
		13.0
		14.0
		15.0
		16.0
		17.0
		18.0
		19.0
		20.0
		21.0
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		25.0
		26.0
		27.0
		28.0
		29.0
		30.0
		31.0
		32.0
		33.0
		34.0
		35.0
		36.0
		37.0
		38.0
		39.0
		40.0
		41.0
		42.0
		43.0
		44.0
		45.0
		46.0
		47.0
		48.0
		49.0
		50.0
		51.0
		52.0
		53.0
		54.0
		55.0

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
BRIDGE NO. 2 ABUTMENT NO. 2
SHEET 19 OF 55 AUGUSTA, MAINE APRIL 1971

152-50

[illegible]

STATE HIGHWAY COMMISSION

U.S. ROUTE NO. 201

OVER

INTERSTATE NO. 95 N.B. & S.B.

IN THE CITY OF

GARDINER

KENNEBEC COUNTY

BRIDGE NO. 2 ABUTMENT NO. 2

SHEET 20 OF 55 AUGUSTA, MAINE APRIL 1958

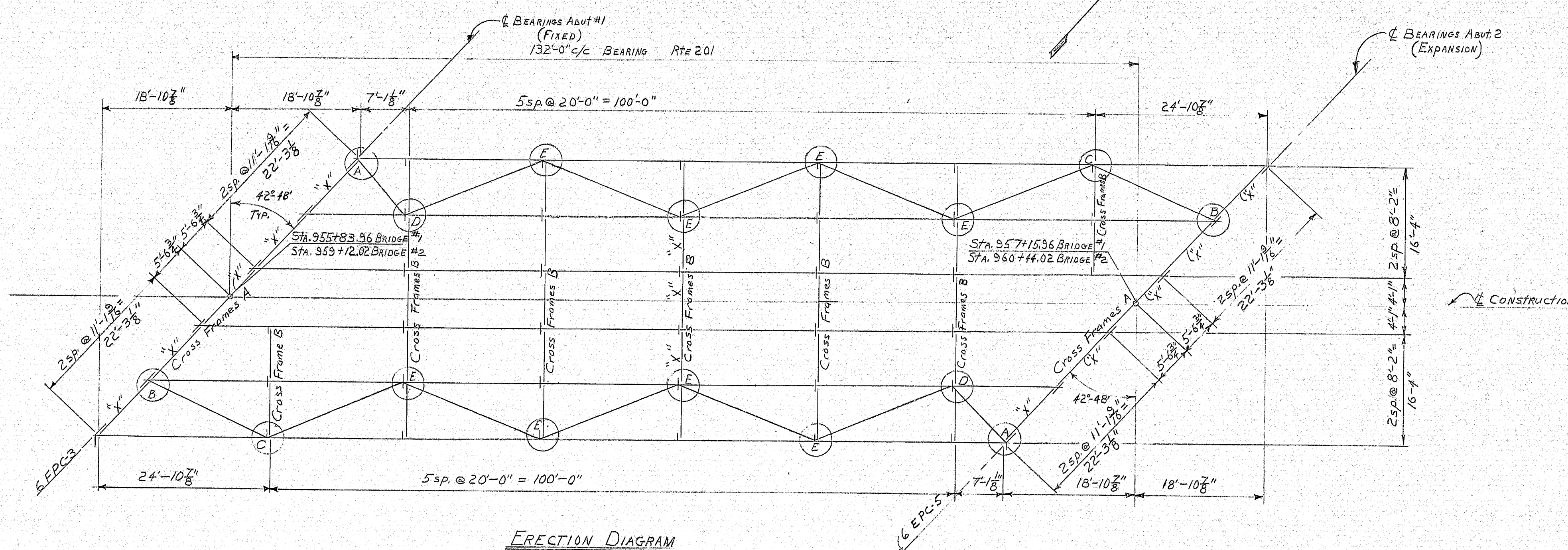
SHEET 20 OF 55. AUGUSTA, MAINE APRIL 19

152-5

S.P.R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195 5(6)74	22	55

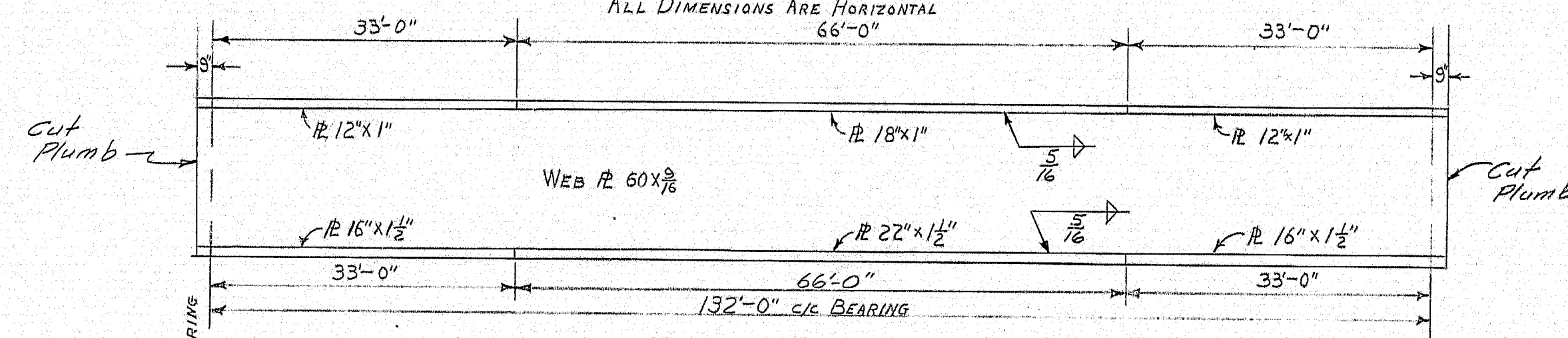
STRUCTURAL STEEL SPECIFICATIONS

1. Camber ordinates, as shown, are computed to compensate for all dead load deflections and for the curvature of the finish grade profile.
2. A maximum of 2 transverse welds in the web per beam per span will be allowed. No transverse web weld shall be within 10'-0" of mid-span.
3. Butt welds in flanges shall not be closer than 1 foot to web transverse welds.
4. Bearing stiffeners shall be plumb after erection and dead loading of the structure.
5. All cross frame or diaphragm connection plates may be plumb or normal to the top flange at the option of the contractor.
6. All bolts shall be 7/8" H.S.
7. The minimum edge distance for all holes shall be 1 1/2" unless shown otherwise.
8. Structural steel:
 - ASTM Designation
 - Beam Webs --- A572 Grade 50
 - Beam Flanges --- A572 Grade 50
 - High Strength Bolts --- ASTM A325
 - Bearing Stiffeners --- A36
 - All Other --- A36
9. Basic Design Stresses:
 - Concrete (n=10) --- 12,000 psi
 - Reinforcing Steel --- 24,000 psi
 - Structural Steel --- 27,000 psi A572
 - 20,000 psi A36
10. Shear Connector Detail see Standard Details BD104-71
11. Drain Details see Standard Details BD104-71 - Drain No. 1
12. Cross Frames see Sheet #23
13. Bearing Pedestals see Standard Details BD101-70
14. Armored Joint see Sheet #27
15. Lateral Bracing Connection see Sheet #24
16. Steel surfaces to be in contact with concrete shall not be painted.



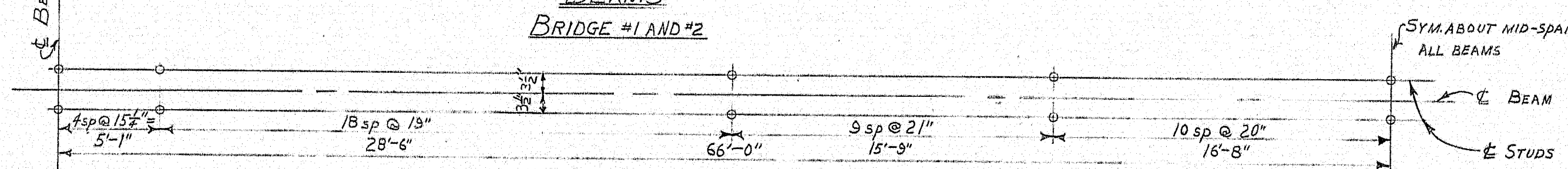
ERECTION DIAGRAM

BRIDGE #1 AND #2
ALL DIMENSIONS ARE HORIZONTAL



BEAMS

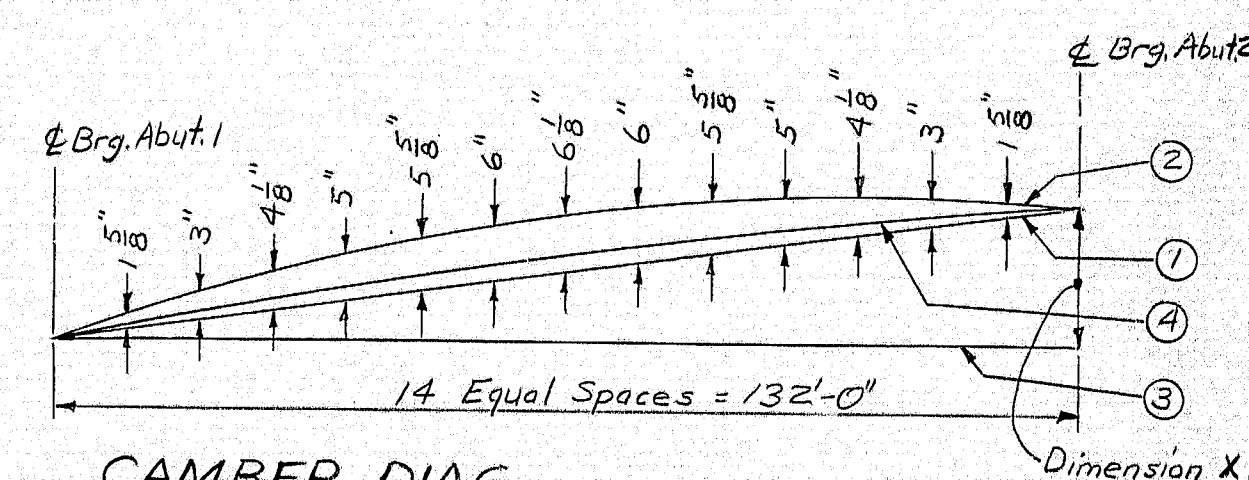
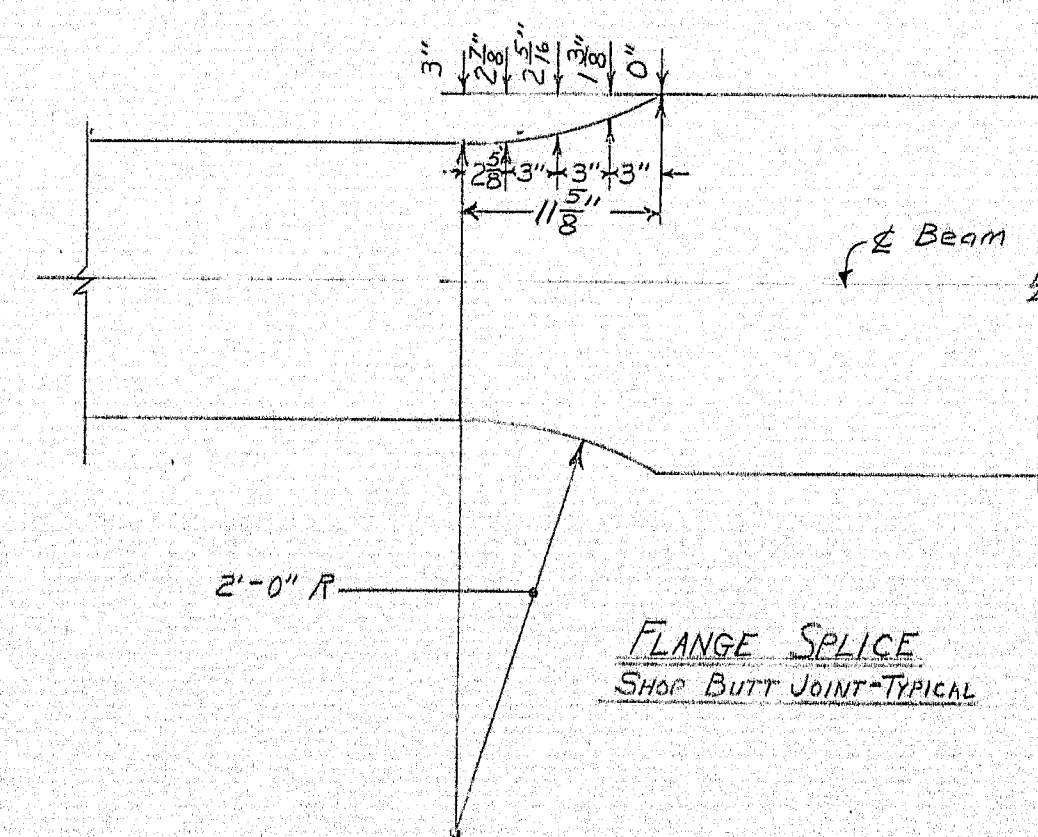
BRIDGE #1 AND #2



SHEAR CONNECTORS

BRIDGE #1 AND #2

TYPICAL
TOTAL STUDS REQ'D = 1992
Total weight = 1,926 lbs.



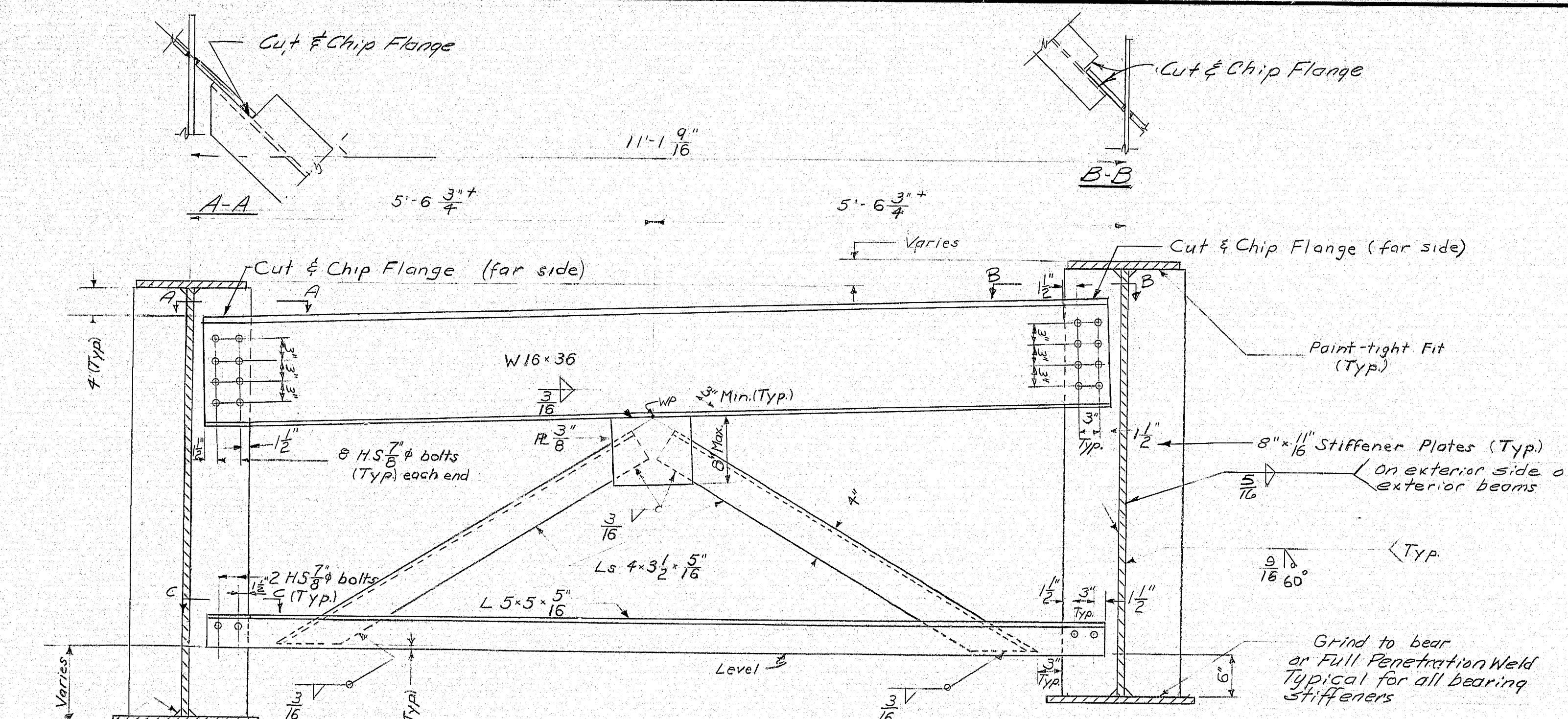
CAMBER DIAG.

Beam	Bridge 1	Bridge 2
B1	3'-1 3/8"	2'-4 1/4"
B2	3'-1 3/4"	2'-4 3/8"
B3	3'-2"	2'-4 1/2"
B4	3'-2 1/4"	2'-4 11/16"
B5	3'-2 1/16"	2'-4 13/16"
B6	3'-2 1/16"	2'-5"

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
STRUCTURAL STEEL
SHEET 22 OF 55 AUGUSTA, MAINE APRIL 1971

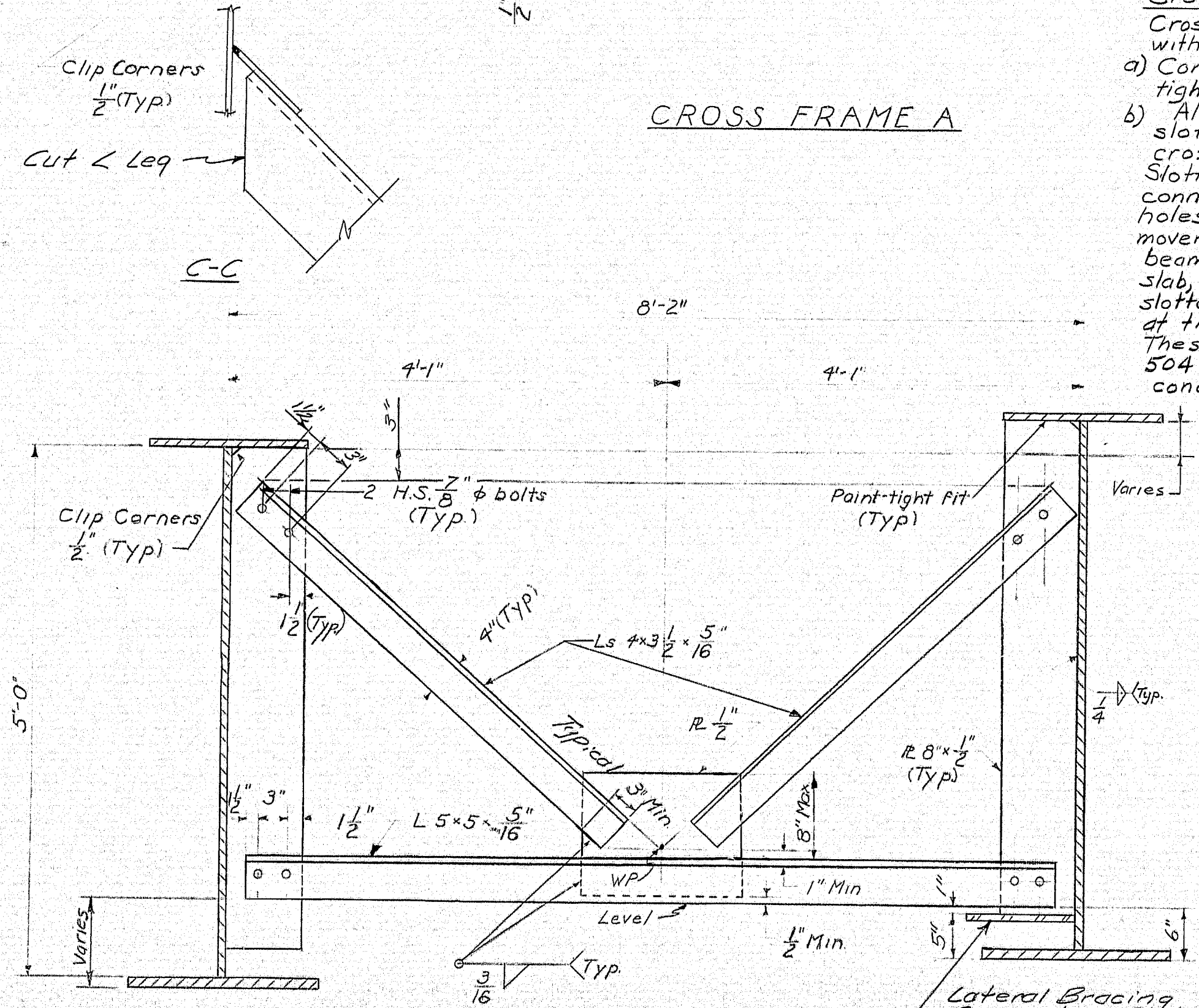
152-53

DESIGN - DATE	BY
DESIGN - DETAILED	EBC
CHECKED	WCE
REVISIONS	
FIELD CHANGES	
APPROVED	



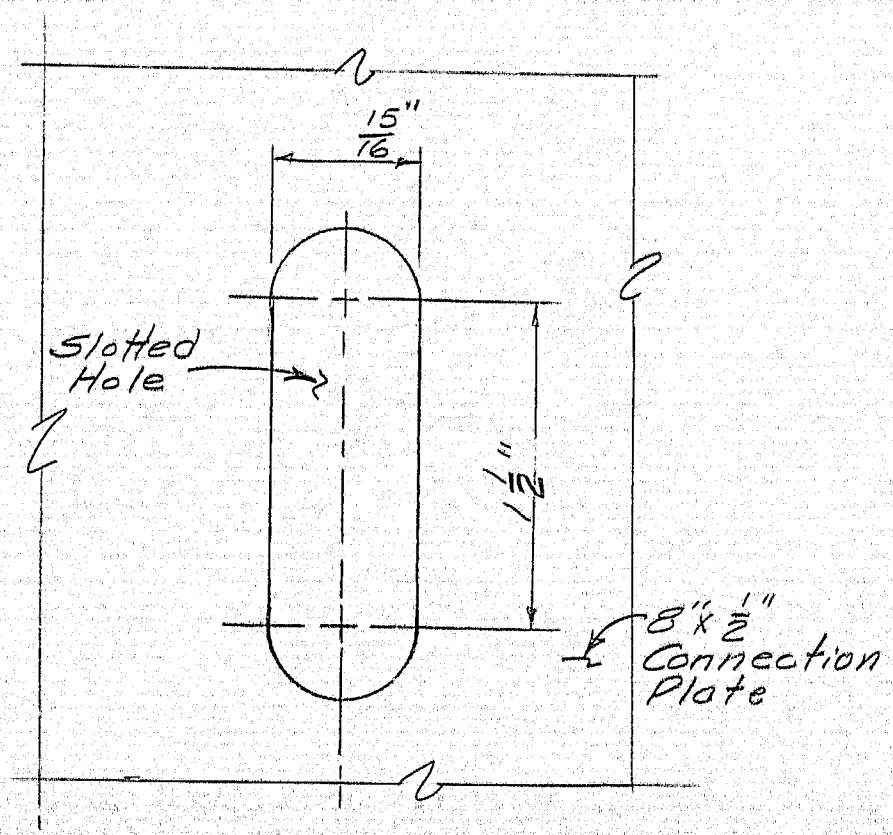
CROSS FRAME A

Cross Frame Note:
Cross frame connection bolts shall be tightened in accordance with section 504, as follows:
a) Connection bolts in cross frames marked "X" shall be tightened before the roadway form work is started.
b) All other cross frames type "B" shall be provided with slotted holes in the connection plates at one end of the cross frame. The slotted holes shall be 1 1/2" long (see Slotted Hole Detail). There shall not be slotted hole connections on both sides of any one beam web. Slotted holes shall be located and oriented to provide for movement due to differential deflection of adjacent beams caused by dead load of concrete structural slab, wearing surface & concrete curbs. The bolts in the slotted hole connections shall be tightened finger tight at the time of steel erection.
These bolts shall be tightened in accordance with Section 504 after all structural slab, wearing surface & curb concrete is in place. All bolts not in slotted connections shall be tightened in accordance with Section 504 at the time the steel is erected & before any concrete is placed for the roadway structural slab.



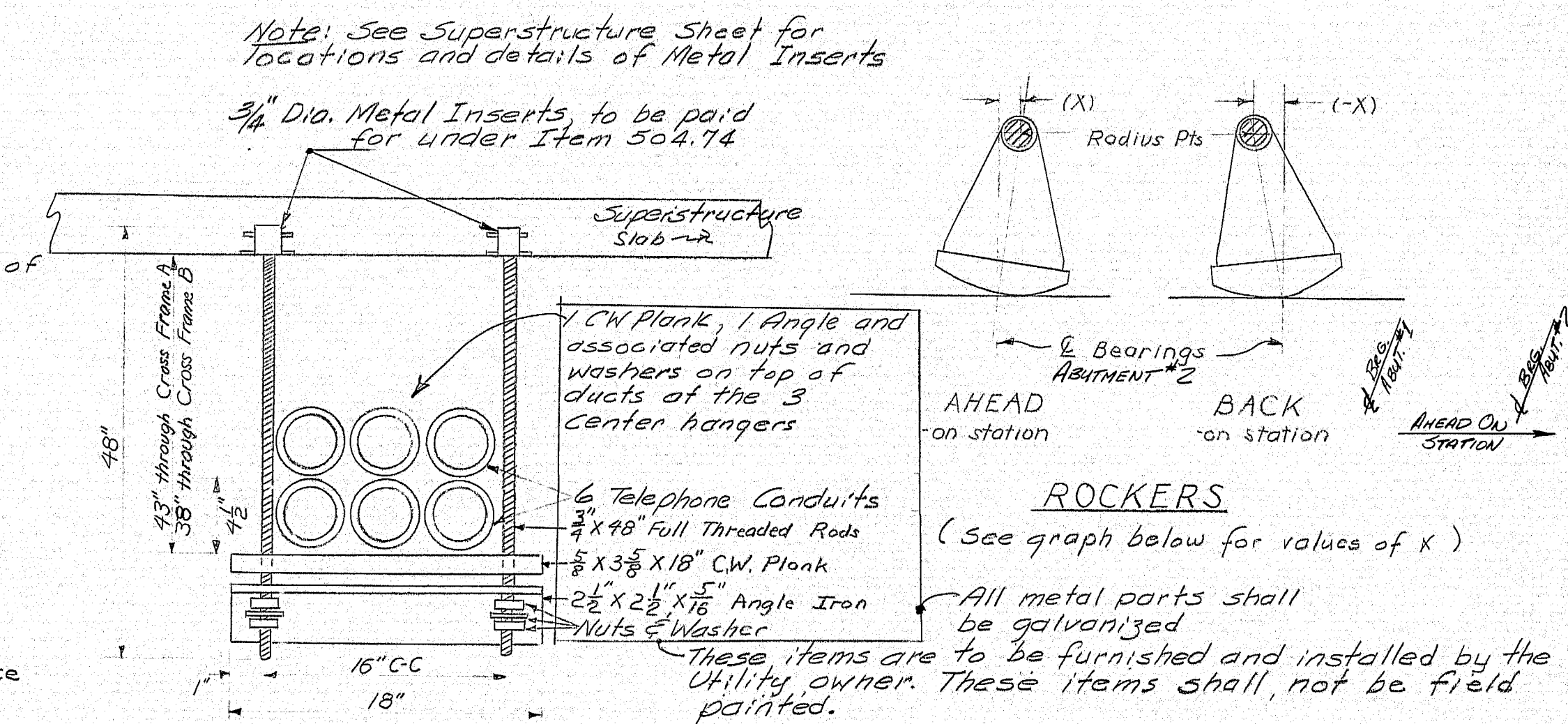
CROSS FRAME B

(All holes for 7/8" H.S. Bolts shall be of 1 3/8" dia.)



SLOTTED HOLE DETAIL

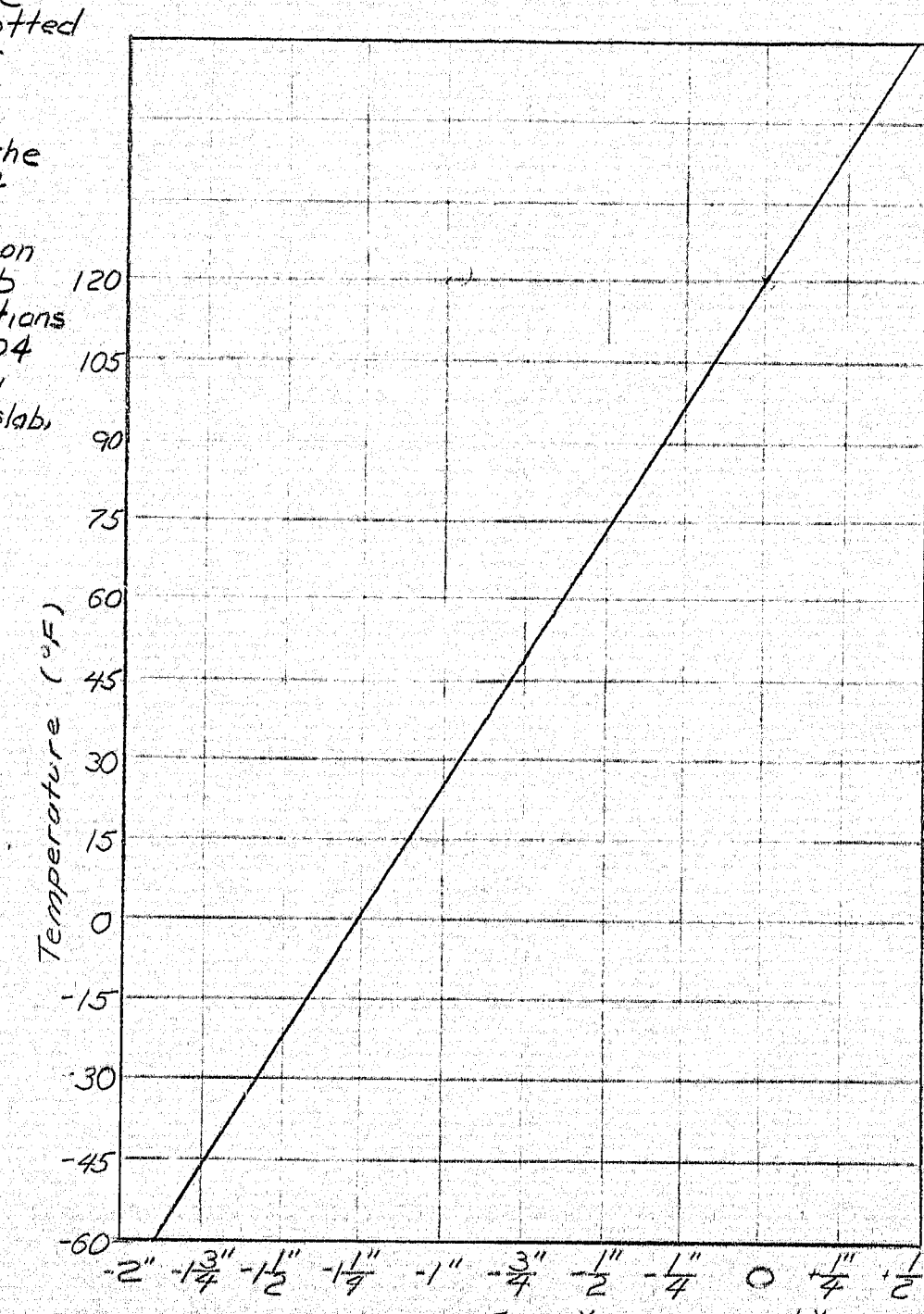
Note:
Slotted holes are to be in the 8" x 1/2" connection plates and slotted holes shall be located & oriented to allow movement due to differential deflection of adjacent beams. See Cross Frame note above.



CONDUIT HANGER

NOTES

1. All steel for cross frame elements, lateral bracing, bearing stiffeners and connection plates, shall be of ASTM designation A36.
2. Expansion bearing rockers to be adjusted as given in "Rocker Setting Data" and sole plates to be welded to beam flanges after all structural steel is in place and before any formwork for the deck slab has been erected.
3. For framing plan see sheet #22.
4. For bearing pedestal details see "Standard Details" sheet #BD101-70.



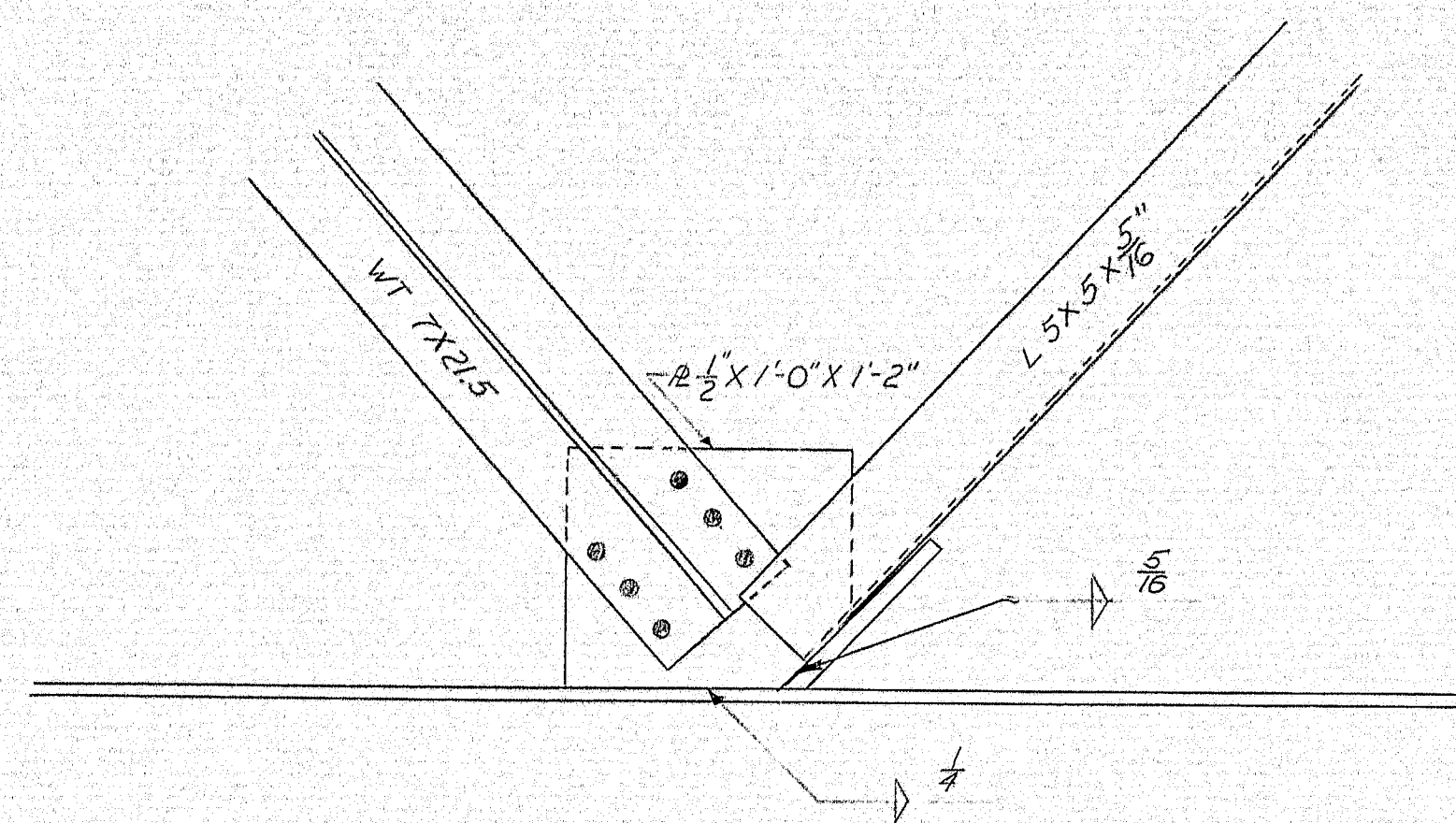
ROCKER SETTING DATA

This graph compensates for temp. and dead load deflection movements. See "ROCKERS" drawing above for direction of +X and -X.

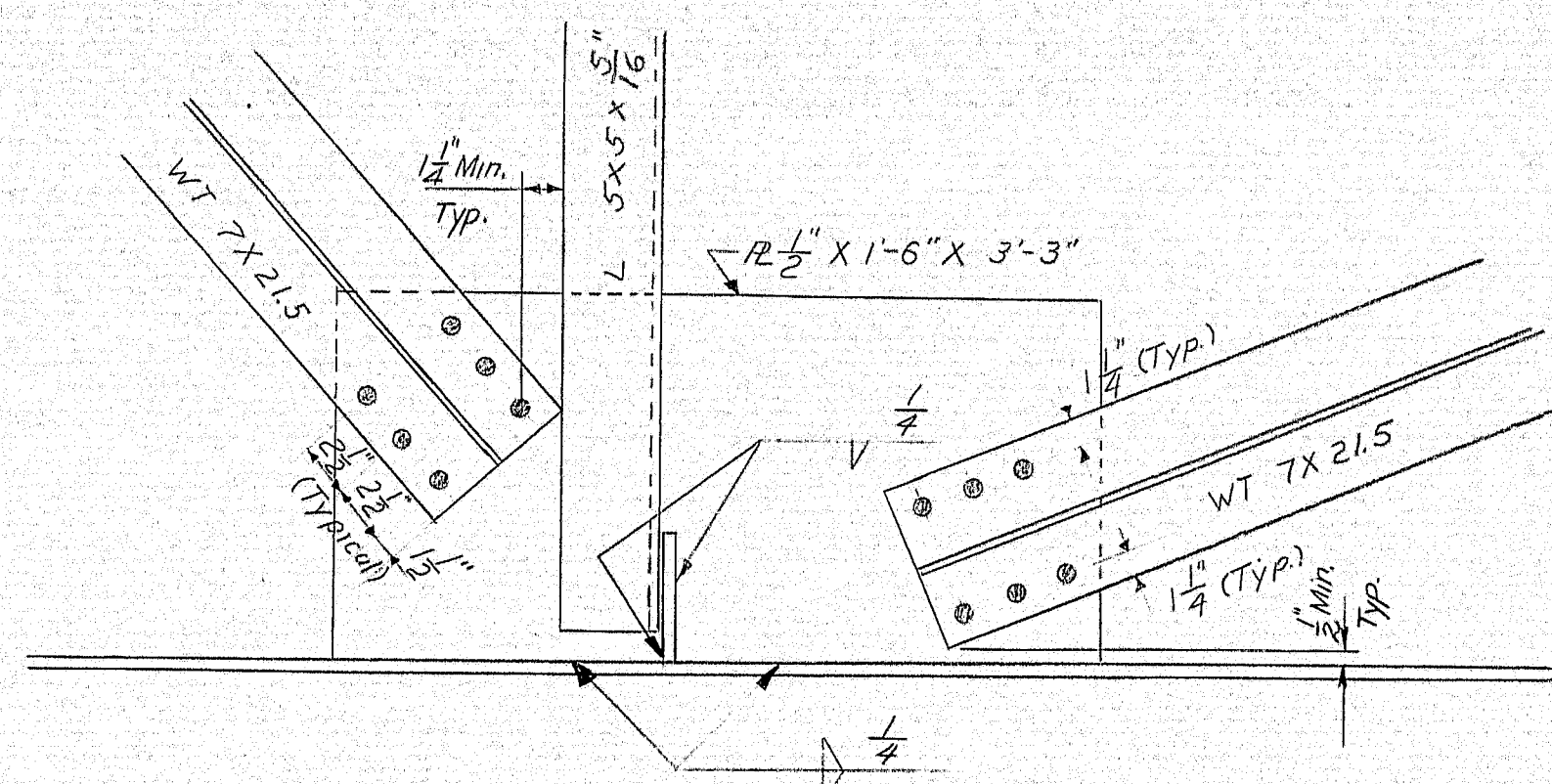
STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
CROSS FRAMES
SHEET 23 OF 55 AUGUSTA, MAINE APRIL 1971

152-54

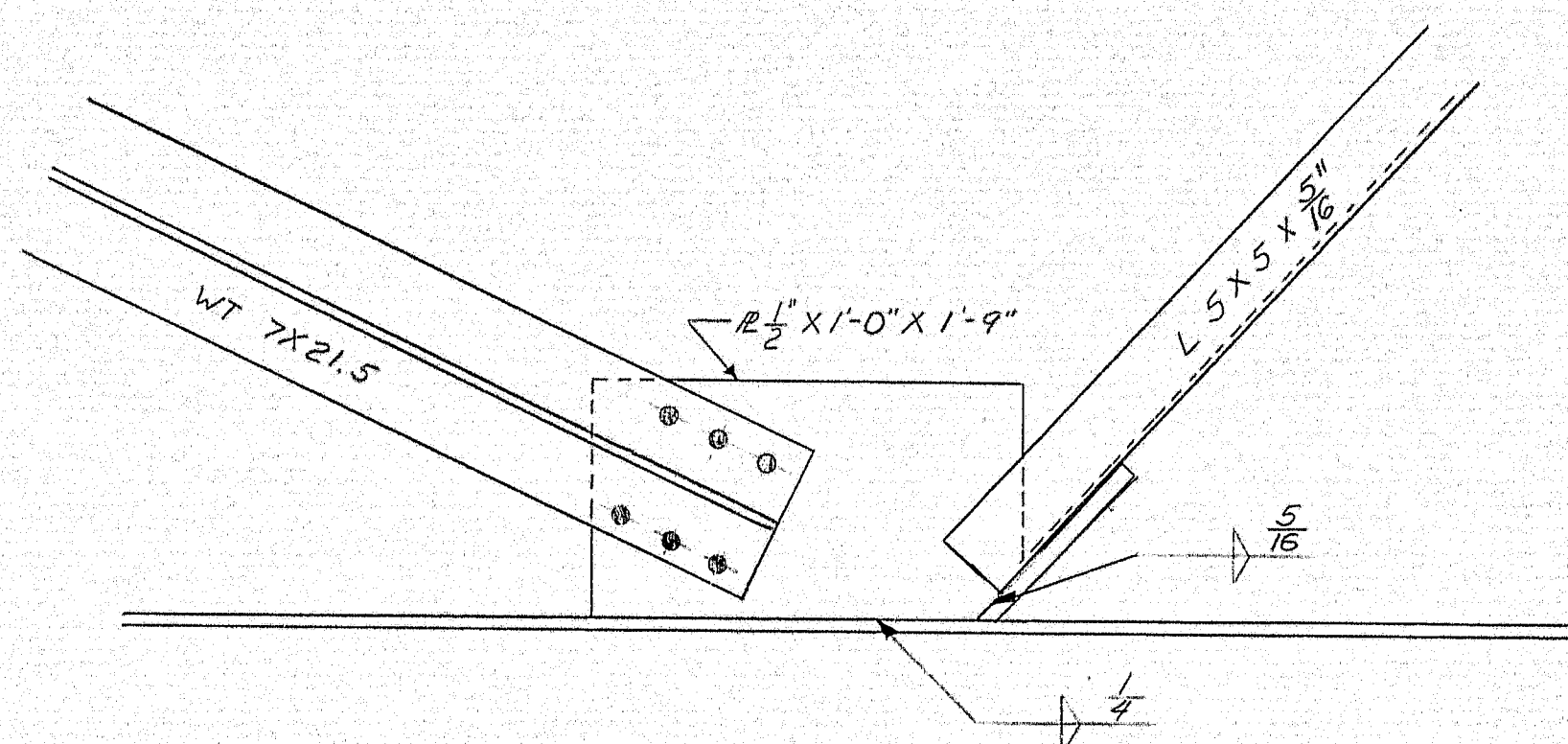
D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	195 5(6)94	24	55



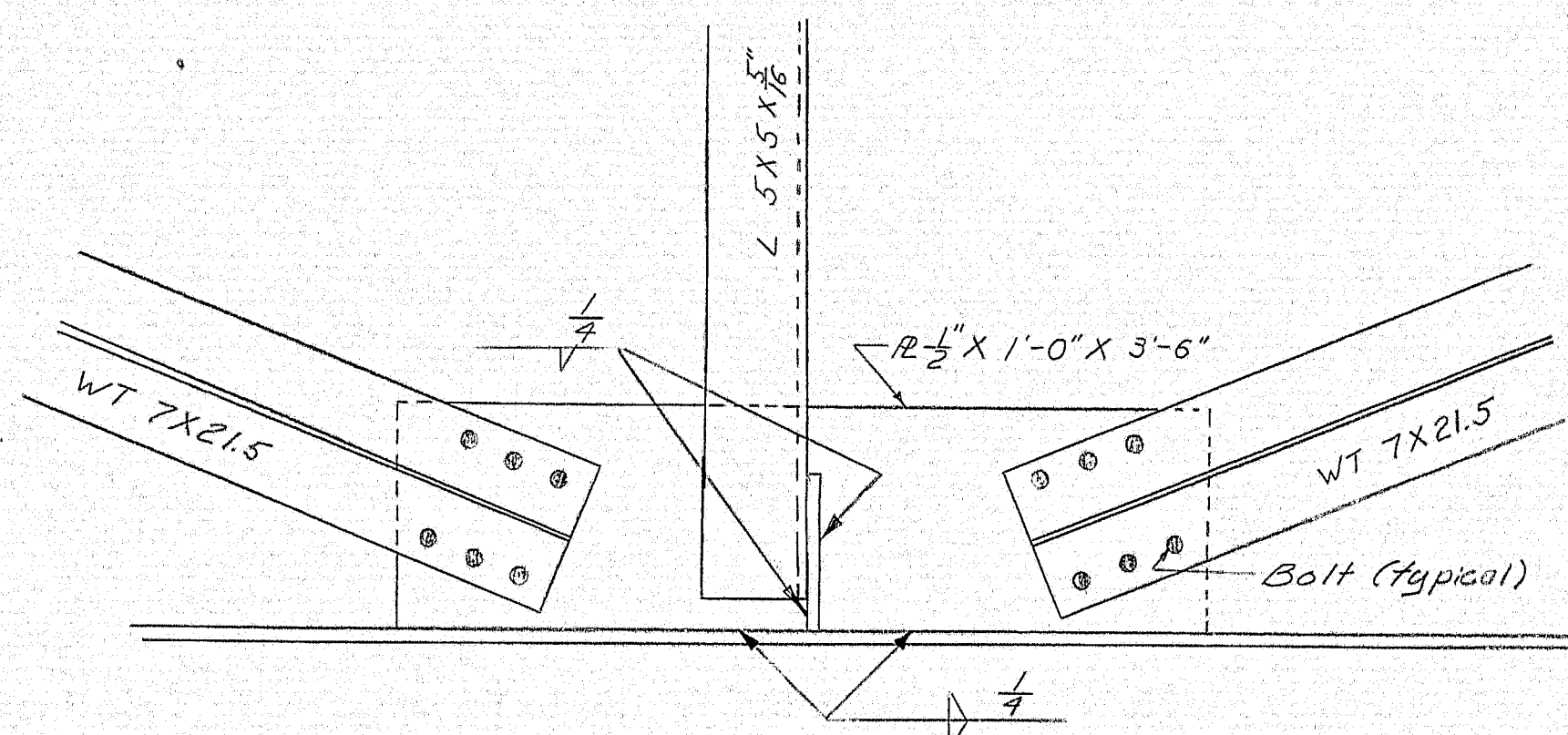
DETAIL A



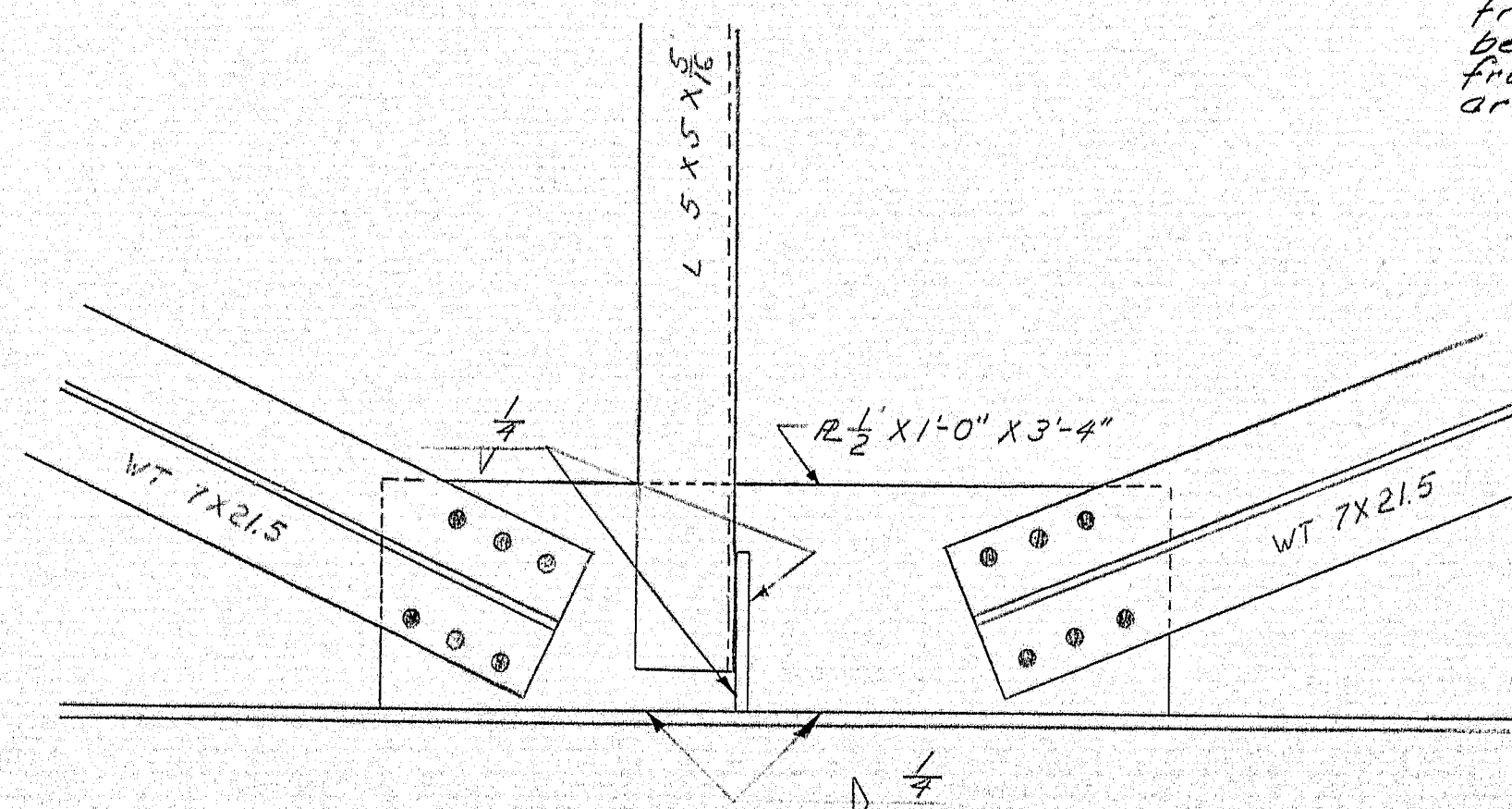
DETAIL D



DETAIL B



DETAIL E



DETAIL C

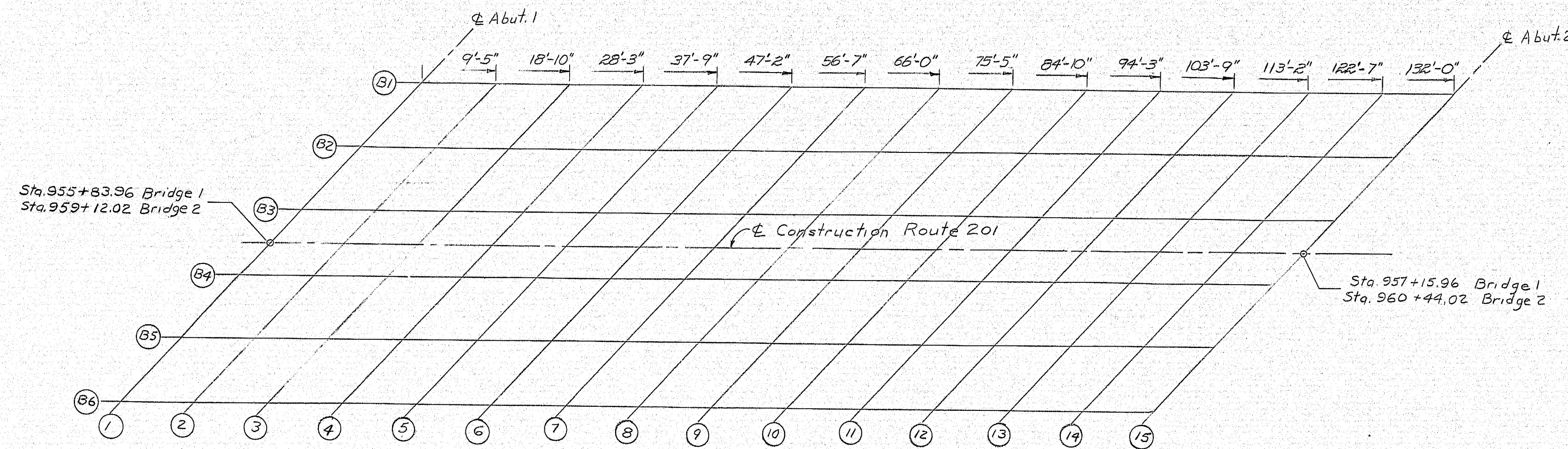
Note: Welds shown are for lateral bracing connection plates to cross frame connection plates and to beam webs. Connections of cross frames to cross frame connection plates are by bolts. See cross frame sheet.

- NOTES:
1. All bolts have minimum edge distance of $1\frac{1}{2}$ " except as noted.
 2. All bolts shall be $\frac{3}{4}$ " H.S. and all holes shall be $\frac{1}{16}$ " ϕ .
 3. Cross Reference See Sheet No. 22 Structural Steel

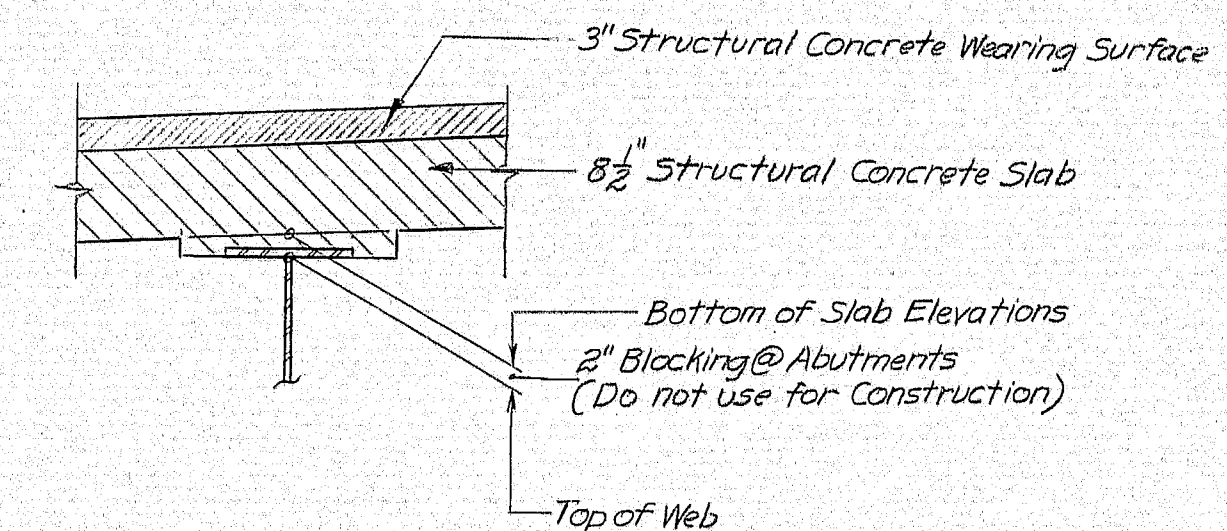
PLANS	DESIGN - DETAILED	CHECKED	REVISIONS	FIELD CHANGES	DATE	BY
					3/5/74	W.K.
						W.C.F.

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
LATERAL BRACING CONNECTION
SHEET 24 OF 55 AUGUSTA, MAINE

152-55



BLOCKING DIAGRAM



BLOCKING DETAIL

NOTE: Bottom of slab elevations have been adjusted to compensate for concrete dead load deflections (Fluid and superimposed) use in conformance with sub-section 502.10(a) of the specifications.

DEAD LOAD DEFLECTIONS (FEET)															
Beams 23#5	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
Steel	.000	.026	.050	.072	.088	.101	.108	.111	.108	.101	.088	.072	.050	.026	.000
Fluid	.000	.081	.157	.222	.273	.311	.335	.343	.335	.311	.273	.222	.157	.081	.000
Superimposed	.000	.019	.037	.053	.066	.075	.081	.083	.081	.075	.066	.053	.037	.019	.000

INTERIOR BEAMS
B2, B3, B4, B5

DEAD LOAD DEFLECTIONS (FEET)															
Beams 1#6	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
Steel	.000	.026	.050	.072	.088	.101	.108	.111	.108	.101	.088	.072	.050	.026	.000
Fluid	.000	.070	.135	.192	.237	.270	.290	.297	.290	.270	.237	.192	.135	.070	.000
Superimposed	.000	.020	.039	.056	.069	.079	.085	.087	.085	.079	.069	.056	.039	.020	.000

EXTERIOR BEAMS
B1 & B6

BOTTOM OF SLAB ELEVATIONS (FEET)															
Beam	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
B1	282.71	282.98	283.24	283.48	283.71	283.93	284.12	284.30	284.45	284.59	284.71	284.82	284.91	284.99	285.07
B2	282.74	283.02	283.29	283.54	283.78	284.00	284.20	284.38	284.53	284.67	284.78	284.89	284.97	285.04	285.11
B3	282.77	283.05	283.32	283.57	283.81	284.03	284.23	284.41	284.57	284.70	284.82	284.92	285.01	285.08	285.14
B4	282.62	282.90	283.17	283.43	283.67	283.89	284.10	284.28	284.43	284.57	284.69	284.79	284.87	284.95	285.01
B5	282.31	282.59	282.86	283.12	283.36	283.58	283.79	283.97	284.13	284.27	284.39	284.49	284.57	284.64	284.71
B6	281.99	282.26	282.53	282.78	283.02	283.24	283.44	283.62	283.76	283.92	284.05	284.16	284.25	284.33	284.41

BRIDGE NO. 2

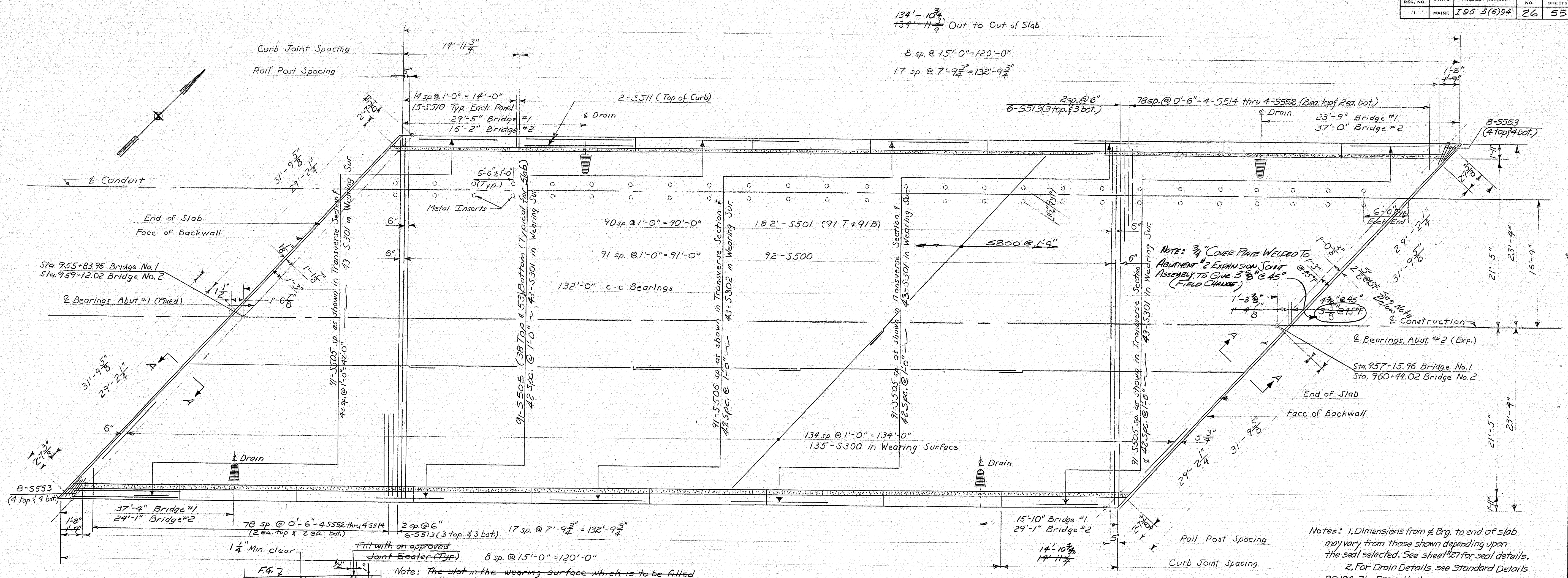
BOTTOM OF SLAB ELEVATIONS (FEET)															
Beam	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
B1	275.54	275.86	276.18	276.49	276.77	277.04	277.30	277.53	277.74	277.94	278.11	278.27	278.42	278.55	278.67
B2	275.52	275.84	276.18	276.50	276.79	277.07	277.33	277.56	277.78	277.97	278.14	278.30	278.43	278.55	278.67
B3	275.50	275.84	276.17	276.48	276.78	277.06	277.31	277.54	277.77	277.96	278.13	278.29	278.43	278.55	278.67
B4	275.31	275.65	275.98	276.29	276.59	276.87	277.13	277.36	277.59	277.78	277.96	278.11	278.25	278.38	278.49
B5	274.95	275.29	275.62	275.94	276.24	276.52	276.78	277.01	277.23	277.43	277.61	277.77	277.91	278.03	278.15
B6	274.58	274.92	275.24	275.55	275.84	276.12	276.38	276.62	276.84	277.04	277.22	277.39	277.54	277.68	277.81

BRIDGE NO. 1

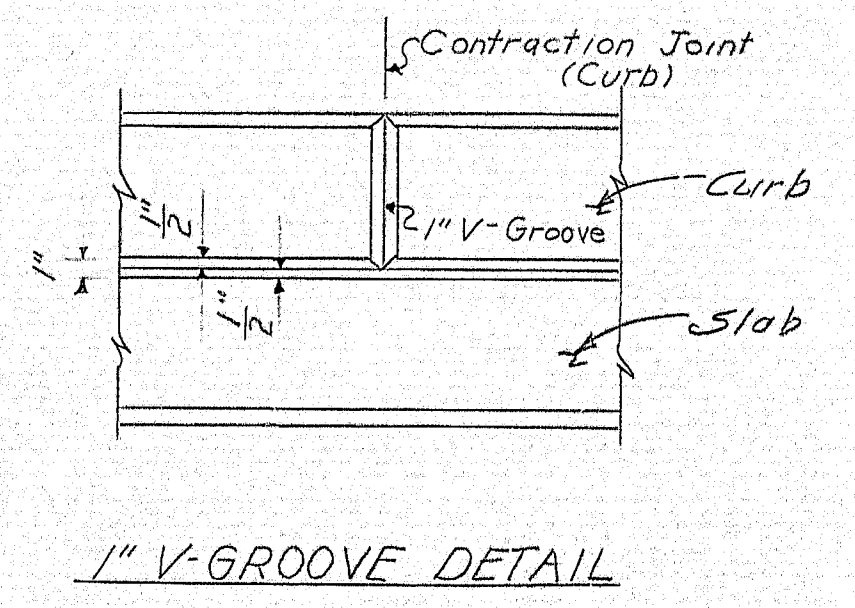
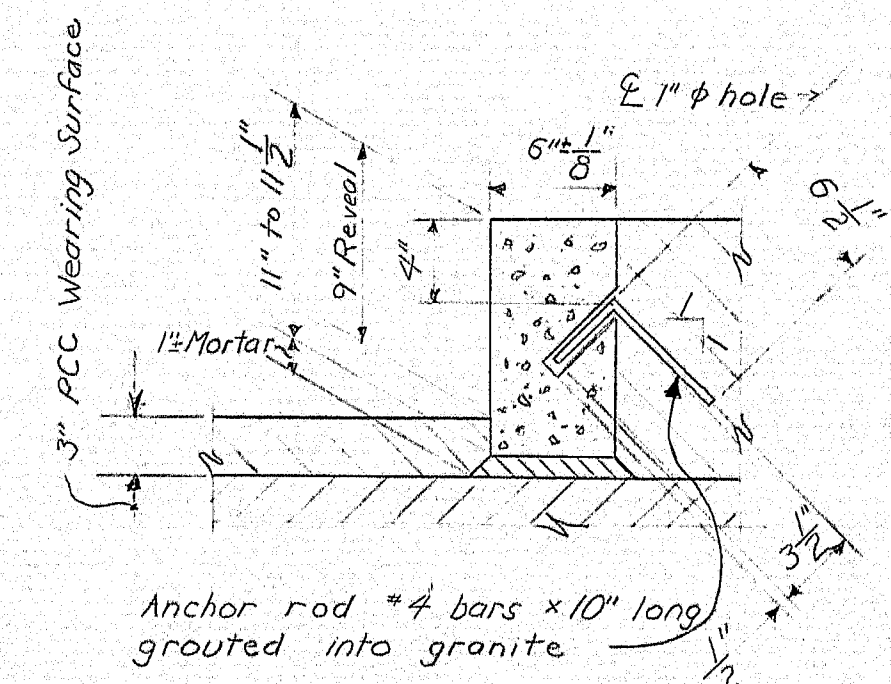
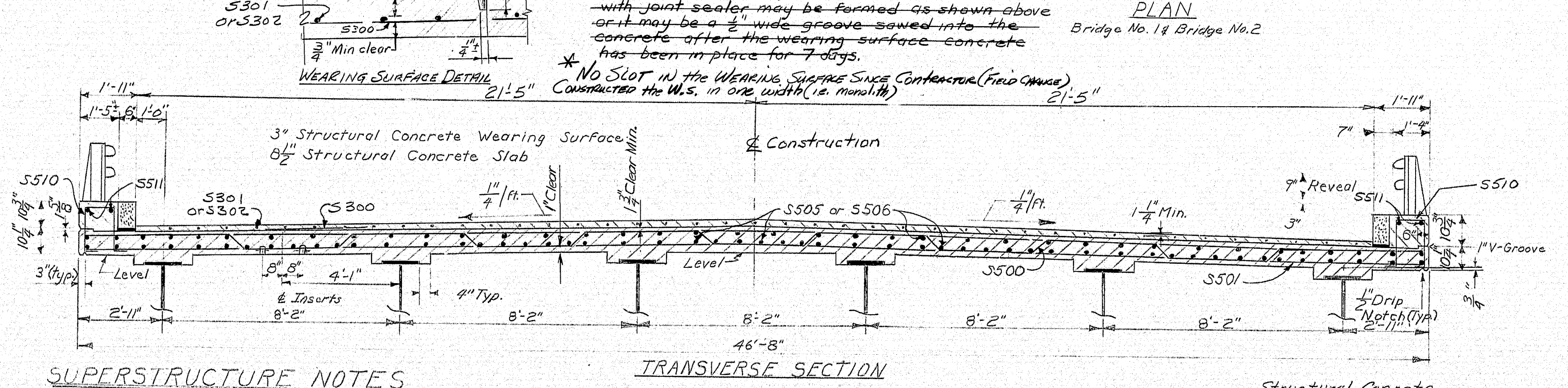
DATE	BY	DESIGN - DETAILED	CHECKED	REVISIONS	FIELD CHANGES
3/6/74		EEC			

STATE HIGHWAY COMMISSION
U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
BOTTOM OF SLAB ELEV. & DEFL.
SHEET 25 OF 55 AUGUSTA, MAINE

152-56

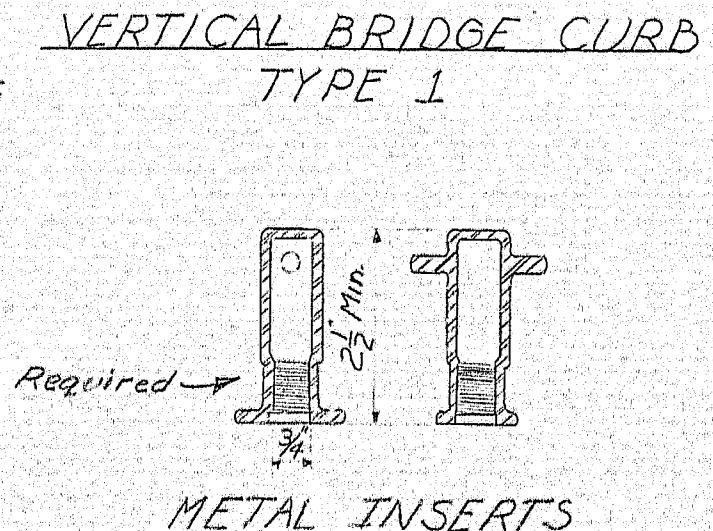
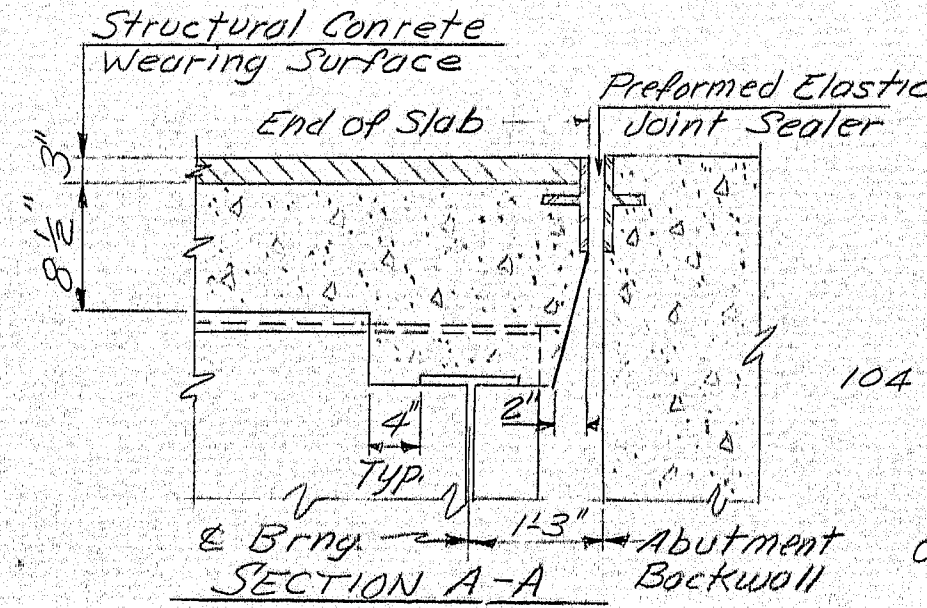


Notes: 1. Dimensions from Δ Brg. to end of slab may vary from those shown depending upon the seal selected. See sheet 27 for seal details.
2. For Drain Details see Standard Details BD 104-71 - Drain No. 1.



- SUPERSTRUCTURE NOTES**
1. Break bond in vertical joints in curbs in a manner approved by the Engineer.
 2. Form a one (1) inch V-groove on the outside faces of curbs at each contraction joint in the curbs.
 3. Set retarding admixtures may be used when authorized by the Engineer and in accordance with the construction specifications.
 4. Provide joints in vertical bridge curb, Type 1 at each contraction joint in concrete curb.
 5. Reinforcing steel shall have a minimum concrete cover of two (2) inches unless otherwise indicated.
 6. All reinforcing steel is to have a minimum of 36 bar diameters splice and/or embedment unless otherwise indicated.
 7. The superstructure concrete shall be Class A.

8. Protective coating for concrete surfaces shall be applied to the following areas: Exposed surface of concrete curbs, facing down to $\frac{1}{2}$ " drip notch, and all exposed surfaces of the concrete wearing surface.
9. Longitudinal reinforcing steel shall be cut to clear drains in a manner approved by the Engineer. Transverse steel shall not be cut.
10. Place metal inserts a minimum of 6" from cross frames.
11. Metal inserts shall be threaded to receive $\frac{3}{8}$ " diameter bolts with National Coarse Threads. Inserts to have a minimum working load of 2500 lbs. and a minimum ultimate strength of 9000 lbs. Inserts to be paid for under Item No. 504.74 See Cross Frame Sheet and details on this sheet for details of Metal Inserts and Conduit Hangers.



METAL INSERTS

Other configurations may be used if approved by the Engineer. See Also Note 11.

STATE HIGHWAY COMMISSION
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OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY
SUPERSTRUCTURES
SHEET 26 OF 55 AUGUSTA, MAINE APRIL 1971

152-57

PLANS	BY	DATE
DESIGN - DETAIL	W/LK	
CHECKED	EBC	
REVISIONS		
FIELD CHANGES	WCE	3/1/74

Preformed Elastic Joint Sealer Notes:

1. The expansion joint of the joint sealer is for illustration only and other configurations may be used. The cross-sectional dimensions of the joint and internal elements must be approved by the Engineer before ordering.
2. The joint sealer to be used shall be capable of providing a minimum movement capacity of a maximum expansion of 1.65 inches at the expansion end, and 0.625 inches at the fixed end.

1. Refer to Standard Details (HD 104-71) for Armored Joint Details not shown.
2. The Armored Joint may be completely assembled in the shop and installed as a unit in the field, or it may be assembled in the field at the contractor's choice.
3. Any clamping assembly that may be used to install the Armored Joint shall be incidental to the contract items.
4. The face of curb chutes in contact with the compression seal to be flush with face of WT 4x2. Surface to be ground smooth after welding.
5. At abutment 1, Bridges 1 & 2, the armored joint shall be set to the narrowest opening recommended by the manufacturer of the seal selected, but not less than 1" perpendicular to the face of the joint.
6. At abutment 2, Bridges 1 & 2, the armored joint shall be set to have an opening at 45° F. equal to the midpoint of the maximum and minimum openings recommended by the manufacturer of the Seal selected. For temperatures other than 45° F. the joint opening shall be adjusted $\frac{1}{8}$ per each 15° F. difference. The maximum allowable opening shall be $\frac{1}{4}$ inches measured normal to the armored joint.

The Contractor shall perform the welding in such a manner as not to cause lead concentrations great enough to damage the seal.

The 1/2" keeper bars under the seal shall not be welded in place until after the type and size of seal has been approved by the Engineer.

STATE HIGHWAY COMMISSION

U.S. ROUTE NO. 201
OVER
INTERSTATE NO. 95 N.B. & S.B.
IN THE CITY OF
GARDINER
KENNEBEC COUNTY

ARMORED JOINT DETAILS
SHEET 27 OF 55 AUGUSTA, MAINE

B. P. N. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOT. SHEETS
1	MAINE	I 95-56194	28	5

NOTES- 1. An alternate to the dimple system for holding the splice bar in position may be used if approved by the Engineer.
 2. Splice bar cross-section to conform to Section B-B on Standard Detail Sheet BD106-69.

Clamp Bars 1 1/2\" x 2\" Bar 3\" x 2\" x 1'-6\" 1/2\" Dia. H.S. Bolts with nuts and washers, 1/16\" Neoprene Sheets and Bolts with washers for Clamp Bars 1 1/2\" to be furnished under this contract and to be erected by others. Payment for furnishing to be included in Item 507.08

Note: All Steel materials, as shown, shall be galvanized to meet the requirements of AASHTO M111. Galvanizing shall be done after steel assembly is fabricated and welded.
 3\" x 2\" Bar and 1/2\" x 10\" x 1'-6\" shall meet ASTM Designation A36

APPROACH SLAB
 (Typ. : 4 Required)
 (By Others)

STATE HIGHWAY COMMISSION	
U.S. ROUTE NO.201	
OVER	
INTERSTATE NO.95 N.B.&S.B.	
IN THE CITY OF	
GARDINER	
KENNEBEC COUNTY	
APPROACH SLABS BR.NO.182 ABUT. NO.182	
SHEET 28 OF 55 AUGUSTA, MAINE	